



DEPARTMENT OF PLANNING AND DEVELOPMENT
CITY OF CHICAGO

October 18, 2022

Lenny D. Asaro
Faegre Drinker Biddle & Reath
320 S. Canal St., Suite 3300
Chicago, IL 60606

Re: Minor change to PD 1212, 3201-3345 W. 31st St., temp. construction fence

Dear Mr. Asaro:

Please be advised that your request for a minor change to Institutional-Business Planned Development No. 1212 ("PD 1212") has been considered by the Department of Planning and Development pursuant to Section 17-13-0611 of the Zoning Ordinance and Statement No. 12 of PD 1212.

Chicago Southwest Development Corp., the sole owner of the PD, is seeking a minor change to allow for the construction of a temporary perimeter 8' high chain link fence with a single gated entry along Kedzie Ave. at the existing curb cut. The temporary fence will be in place until the completion of construction of the New Saint Anthony Hospital and other components of Phase I of the project as described in PD 1212. A specification drawing of the proposed fence is attached.

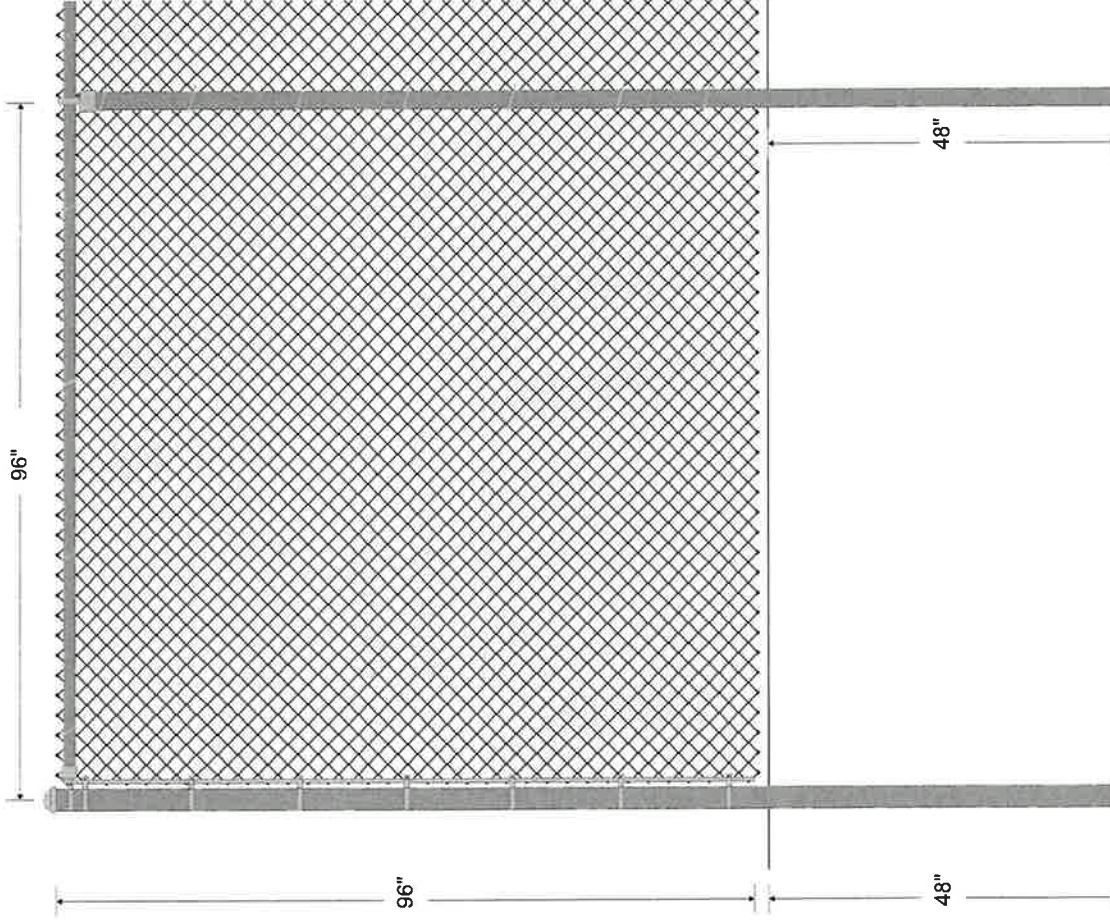
The Department of Planning and Development has determined that allowing the temporary construction fence will not create an adverse impact on the Planned Development or surrounding neighborhood, will not result in an increase in the bulk or density, and will not change the character of the development, and therefore, would constitute a minor change.

Accordingly, pursuant to the authority granted by the Chicago Zoning Ordinance and PD 1212, I hereby approve the foregoing minor change, but no other changes to this Planned Development. This minor change is valid for twelve (12) months from the date of this letter unless action to implement the minor change is commenced within such time period and thereafter diligently pursued to completion, including, if applicable, construction consistent with the minor change as authorized by a building permit. If action to implement the minor change, including construction, does not begin within the time set forth, or does not proceed with reasonable diligence, then the approval will lapse and become null and void.

Sincerely,

Noah Szafraniec
Assistant Commissioner, Plan Commission

C: Mike Marmo, Janice Hill, Main file



FABRIC: 96" 9 GA. GALVANIZED (2" Mesh) KK CHAIN LINK FABRIC.

TOP RAIL: 1 5/8" O.D. SS-40 PIPE, 1.84 lbs. per foot. Top rail 21' in length, joined with 1 5/8" SLEEVE.

LINE POST: 2 1/2" O.D. SS-40 PIPE CUT POST, 2.21 lbs. per foot. Line posts set 8' on center maximum spacing.

TERMINAL POST: 3" O.D. SS-40 PIPE CUT POST, 2.21 lbs. per foot.

FITTINGS: BEVELED BRACE BAND & CARRIAGE BOLT, OFFSET PS RAIL-END, PS EYE-TOP, DOME PS CAP, 3/4" GALV STRIP TENSION BAR, REGULAR TENSION BAND & CARRIAGE BOLT.

TIE WIRE: 6 1/2" 9 GA. ALUMINUM TIE WIRE spaced 15" on center for line posts & 24" on center for rails.



Complete Northern Illinois Fence
320 W LINCOLN HIGHWAY
Cortland, IL 60112
Office: 815-756-3561 Fax: 815-756-7920

SWDC
Cortland, IL

LINE OF FENCE

Drawn: 09/29/2022
File:

Reclassification Of Area Shown On Map No. 26-F.
(Application No. A-8723)
(Common Address: 400 W. 107th St.)

[O2021-3730]

Be It Ordained by the City Council of the City of Chicago:

SECTION 1. Title 17 of the Municipal Code of Chicago, the Chicago Zoning Ordinance, is hereby amended by changing all of the M1-1 Limited Manufacturing/Business Park District symbols and indications as shown on Map Number 26-F in the area bounded by:

South Eggleston Avenue; West 106th Street; the alley next east of and parallel to South Eggleston Avenue; a line 100 feet east of and parallel to South Eggleston Avenue; the east/west alley north of West 107th Street; and West 107th Street,

to those of an RS2 Residential Single-Unit (Detached House) District.

SECTION 2. This ordinance shall be in force and effect from and after its passage and due publication.

Reclassification Of Area Shown On Map No. 121-B **IBPD 1212, AA**
(As Amended)

(Application No. 20869)

(Common Address: 3201 -- 3345 W. 31st St. And 3100 -- 3258 S. Kedzie Ave.)

[SO2021-5158]

Be It Ordained by the City Council of the City of Chicago:

SECTION 1. That the Chicago Zoning Ordinance be amended by changing all of the C3-1 Commercial, Manufacturing and Employment District, M3-3 Heavy Industry District and Institutional-Business Planned Development Number 1212 symbols and indications as shown on Map Number 121-B in the area bounded by:

West 31st Street; South Kedzie Avenue; a line 1,278.99 feet south of and parallel to West 31st Street; a line 1,044.68 feet west of and parallel to South Kedzie Avenue; a line 375.38 feet south of and parallel to West 31st Street; and a line 1,071.78 feet west of and parallel to South Kedzie Avenue,

to those of a C2-5 Motor Vehicle-Related Commercial District.

SECTION 2. That the Chicago Zoning Ordinance be amended by changing all of the C2-5 Motor Vehicle-Related Commercial District symbols and indications as shown on Map Number 121-B in the area bounded by:

West 31st Street; South Kedzie Avenue; a line 1,278.99 feet south of and parallel to West 31st Street; a line 1,044.68 feet west of and parallel to South Kedzie Avenue; a line 375.38 feet south of and parallel to West 31st Street; and a line 1,071.78 feet west of and parallel to South Kedzie Avenue,

to those of Institutional-Business Planned Development Number 1212, as amended, which is hereby established in the area described above subject to such use and bulk regulations as are set forth in the Plan of Development herewith attached and made a part thereof and to no others.

SECTION 3. This ordinance shall be in force and effect from and after its passage and due publication.

Plan of Development Statements referred to in this ordinance read as follows:

Amended Institutional-Business Planned Development No. 1212.

Planned Development Statements.

1. The area delineated herein as Amended Institutional-Business Planned Development Number 1212 ("Planned Development") consists of approximately 1,258,598 square feet of property which is depicted on the attached Planned Development Boundary and Property Line Map ("Property"). Chicago Southwest Development Corporation, an Illinois not-for-profit corporation is the "Applicant" for this Planned Development. The Applicant is the owner of part of the Property, said part commonly known as 3244 -- 3250 South Kedzie Avenue, 3200 South Kedzie Avenue, 3230 and 3354 West 31st Street and 3345 West 31st Street ("Applicant Property"). The Board of Education of the City of Chicago ("BOE") is the beneficial owner of the remainder of the Property, said remainder part commonly known as 3201 -- 3345 West 31st Street and 3100 -- 3150 South Kedzie Avenue ("BOE Property"). The BOE has agreed to convey the BOE Property to the City and the Chicago City Council approved a redevelopment agreement between the City and Applicant ("RDA") pursuant to which the City agreed to convey the BOE Property to the Applicant so that the Applicant can rezone and redevelop the BOE Property consistent with the uses in Subareas A and C described in this Planned Development. Based on the RDA, the BOE and City authorize this Planned Development.

2. The requirements, obligations and conditions contained within this Planned Development shall be binding upon the Applicant, its successors and assigns and, if different than the Applicant, the legal titleholders and any ground lessors. All rights granted hereunder to the Applicant shall inure to the benefit of the Applicant's successors and assigns and, if different than the Applicant, the legal titleholder and any ground lessors. Furthermore, pursuant to the requirements of Section 17-8-0400 of the Chicago Zoning Ordinance, the Property, at the time of application for amendments, modifications or changes (administrative, legislative or otherwise) to this Planned Development are made, shall be under single ownership or designated control. Single designated control is defined in Section 17-8-0400. Provided, however, pursuant to Section 17-8-0400, after the adoption of an ordinance wherein the Property is divided into specifically delineated subareas or subparcels, each having its own bulk and density standards, or similar subarea specific or subparcel specific development controls or requirements, the owners of or designated controlling party for each subarea may seek amendments, changes, or modifications for that subarea without the consent of the owners or designated controlling party of the other subareas. Furthermore, pursuant to Section 17-8-0400, in no instance shall the owner or designated controlling party of a subarea be permitted to unilaterally seek an amendment, change or modification that would reduce any bulk, density, parking or similar development requirement generally available or applicable to all subareas, such as any unused bulk or density rights, or which would materially adversely reduce another subarea owner's right of access, or which would materially adversely reduce open space, walkways, or similar design requirements applicable to one or more subareas, or which would render another subarea a nonconforming use.
3. All applicable official reviews, approvals or permits are required to be obtained by the Applicant or its successors, assignees or grantees. Any dedication or vacation of streets or alleys or grants of easements or any adjustment of the right-of-way shall require a separate submittal to the Department of Transportation on behalf of the Applicant or its successors, assigns or grantees.

Any requests for grants of privilege, or any items encroaching on the public way, shall be in compliance with this Planned Development.

Ingress or egress shall be pursuant to this Planned Development and may be subject to the review and approval of the Departments of Planning and Development and Transportation. Closure of all or any public street or alley during demolition or construction shall be subject to the review and approval of the Department of Transportation.

Pursuant to a negotiated and executed Perimeter Restoration Agreement ("Agreement") by and between the Department of Transportation's Division of Infrastructure Management and the Applicant, the Applicant shall provide improvements and restoration of all public way adjacent to the Property, which may include, but not be limited to, the following as shall be reviewed and determined by the Department of Transportation's Division of Infrastructure Management:

- Full width of streets
- Full width of alleys
- Curb and gutter
- Pavement markings
- Sidewalks
- ADA crosswalk ramps
- Parkway and landscaping

The Agreement must be executed prior to any Department of Transportation and Planned Development Part II review permitting. The Agreement shall reflect that all work must comply with current Rules and Regulations and must be designed and constructed in accordance with the Department of Transportation's Construction Standards for Work in the Public Way and in compliance with the Municipal Code of Chicago Chapter 10-20. Design of said improvements should follow the Department of Transportation's Rules and Regulations for Construction in the Public Way as well as The Street and Site Plan Design Guidelines. Any variation in scope or design of public way improvements and restoration must be approved by the Department of Transportation.

The Applicant commits to fully fund and commence construction of the following infrastructure improvements prior to issuance of a Certificate of Occupancy. All infrastructure improvements will be subject to review and approval by the Department of Transportation and must be designed and constructed in accordance with the Department of Transportation's Construction Standards for Work in the Public Way and in compliance with the Municipal Code of Chicago:

- Installation of traffic signals at the intersection of 31st Street, Spaulding Avenue and the access drive, with full ADA compliant crosswalks.
- Installation of traffic signals at the intersection of Kedzie Avenue/access drive, with full ADA compliant crosswalks.
- Fully modernize the traffic signal at 31st Street and Kedzie Avenue, by removing and replacing all signal equipment. The new signal shall include actuated left-turn arrows on all approaches, countdown pedestrian signals, and all curb ramps shall be upgraded to ensure ADA compliance.

4. This Planned Development consists of 17 Statements; a Bulk Regulations Table; an Existing Zoning Map; an Existing Land-Use Map; an Aerial Map; a Planned Development Boundary and Property Line Map; a Typical Right-of-Way Section; Site Plan; Site Plan -- Phase 1; a Subarea Map; a Subarea Plan; a Phasing Plan; Landscape Plan; Landscape Plan -- Phase 1; a Green Roof Plan; Building Elevations (North, South, East and West); Chicago Builds Green form prepared by HDR Architecture, Inc. and dated January 20, 2022; and a Traffic Report and a Shuttle Report prepared by Kimley Horn and dated (date of Plan Commission presentation), submitted herein. Full-sized copies of the Site Plan, Landscape Plan and Building Elevations are on file with the Department of Planning and Development. In any instance where a provision of this Planned Development conflicts with the Chicago Building Code, the Building Code shall control. This Planned Development conforms to the intent and purpose of the Chicago Zoning Ordinance, and all requirements thereto, and satisfies the established criteria for approval as a Planned Development. In case of a conflict between the terms of this Planned Development and the Chicago Zoning Ordinance, this Planned Development shall control.
5. In each of the following subareas, the following uses shall be permitted in this Planned Development:

Subarea A:

Hospital; parks and recreation: community centers, recreation buildings and similar assembly use; parks and recreation: community garden; artist work or sales space; business support services: employment agencies; eating and drinking establishments: restaurant, limited; eating and drinking establishments: outdoor patio (if located on a rooftop); eating and drinking establishments: outdoor patio (if located at grade level); entertainment and spectator sports: indoor special event including incidental liquor sales; entertainment and spectator sports: banquet or meeting halls; financial services: bank, savings bank, savings and loan association, currency exchange and credit union; financial services: automated teller machine facility; flea market; food and beverage retail sales: liquor sales (as accessory use); medical service; office but excluding electronic data storage center; personal service: hair salon, nail salon, or barbershop; personal service: massage establishment; repair or laundry service, consumer: dry cleaning drop-off or pick-up (no on-premise plant); retail sales, general; sports and recreation, participant: outdoor; sports and recreation, participant: indoor; vehicle sales and service: light equipment sales/rental, indoor; manufacturing, production and industrial services: artisan; manufacturing, production and industrial services: limited (catering and shared kitchen only); manufacturing, production and industrial services: limited; wireless communication facilities: co-located; wireless communication facilities: freestanding (towers); and related accessory uses including electronic data storage center only to serve as accessory to the hospital, medical service and office uses.

Subarea B:

Colleges and universities; artist work or sales space; drive-through facility; eating and drinking establishments: restaurant, limited; eating and drinking establishments: restaurant, general; eating and drinking establishments: tavern; eating and drinking establishments: outdoor patio (if located at grade level); entertainment and spectator sports: indoor special event including incidental liquor sales; entertainment and spectator sports: banquet or meeting halls; financial services: bank, savings bank, savings and loan association, currency exchange and credit union; financial services: automated teller machine facility; food and beverage retail sales: liquor sales (as accessory use); medical service; office but excluding electronic data storage center; personal service: hair salon, nail salon, or barbershop; personal service: massage establishment; repair or laundry service, consumer: dry cleaning drop-off or pick-up (no on-premise plant); retail sales, general; wireless communication facilities: co-located; wireless communication facilities: freestanding (towers); and related accessory uses.

Subarea C:

Day care; parks and recreation: community garden; eating and drinking establishments: restaurant, limited; eating and drinking establishments: restaurant, general; eating and drinking establishments: outdoor patio (if located at grade level); retail sales, general; repair or laundry service, consumer: dry cleaning drop-off or pick-up (no on-premise plant); sports and recreation, participant: children's play center; wireless communication facilities: co-located; and related accessory uses.

Subarea D:

Artist work or sales space; drive-through facility; eating and drinking establishments: restaurant, limited; eating and drinking establishments: restaurant, general; eating and drinking establishments: tavern; eating and drinking establishments: outdoor patio (if located at grade level); entertainment and spectator sports: indoor special event including incidental liquor sales; financial services: bank, savings bank, savings and loan association, currency exchange and credit union; financial services: automated teller machine facility; food and beverage retail sales: liquor sales (as accessory use); medical service; office but excluding electronic data storage center; personal service: hair salon, nail salon, or barbershop; personal service: massage establishment; retail sales, general; manufacturing, production and industrial services: artisan; manufacturing, production and industrial services: limited (catering and shared kitchen only); manufacturing, production and industrial services: limited; wireless communication facilities: co-located; and related accessory uses.

6. On-premises signs and temporary signs, such as construction and marketing signs, shall be permitted within this Planned Development, subject to the review and approval of the Department of Planning and Development. Off-premises signs are prohibited within the boundary of this Planned Development.
7. For purposes of height measurement, the definitions in the Chicago Zoning Ordinance shall apply. The height of any building shall also be subject to height limitations, if any, established by the Federal Aviation Administration.
8. The maximum permitted floor area ratio ("FAR") for the Property shall be in accordance with the attached Bulk Regulations and Data Table. For the purpose of FAR calculations and measurements, the definitions in the Chicago Zoning Ordinance shall apply. The permitted FAR identified in the Bulk Regulations and Data Table has been determined using a net site area of 1,258,598 square feet and a base FAR of 2.0.
9. Upon review and determination, Part II review, pursuant to Section 17-13-0610, a Part II review fee shall be assessed by the Department of Planning and Development. The fee, as determined by staff at the time, is final and binding on the Applicant and must be paid to the Department of Revenue prior to the issuance of any Part II approval.
10. The Site and Landscape Plans shall be in substantial conformance with the Landscape Ordinance and any other corresponding regulations and guidelines, including Section 17-13-0800. Final landscape plan review and approval will be by the Department of Planning and Development. Any interim reviews associated with site plan review or Part II reviews, are conditional until final Part II approval.
11. The Applicant shall comply with Rules and Regulations for the Maintenance of Stockpiles promulgated by the Commissioners of the Departments of Streets and Sanitation, Fleet and Facility Management and Buildings, under Section 13-32-085, or any other provision of the Municipal Code of Chicago.
12. The terms and conditions of development under this Planned Development may be modified administratively, pursuant to Section 17-13-0611-A, by the Zoning Administrator upon the application for such a modification by the Applicant, its successors and assigns and, if different than the Applicant, the legal titleholders and any ground lessors.
13. The Applicant acknowledges that it is in the public interest to design, construct and maintain the project in a manner which promotes, enables and maximizes universal access throughout the Property. Plans for all buildings and improvements on the Property shall be reviewed and approved by the Mayor's Office for People with Disabilities to ensure compliance with all applicable laws and regulations related to access for persons with disabilities and to promote the highest standard of accessibility.

14. The Applicant acknowledges that it is in the public interest to design, construct, renovate and maintain all buildings in a manner that provides healthier indoor environments, reduces operating costs and conserves energy and natural resources. The Applicant shall obtain the number of points necessary to meet the requirements of the Chicago Sustainable Development Policy, in effect at the time the Part II review process is initiated for each improvement that is subject to the aforementioned policy and must provide documentation verifying compliance.
15. The Applicant acknowledges that it is the policy of the City to maximize opportunities for Minority- and Women-owned Business Enterprises ("M/WBEs") and city residents to compete for contracts and jobs on construction projects approved through the planned development process. To assist the City in promoting and tracking such M/WBE and city resident participation, an applicant for planned development approval shall provide information at three points in the City approval process. First, the applicant must submit to the Department of Planning and Development, as part of its application for planned development approval, an M/WBE Participation Proposal. The M/WBE Participation Proposal must identify the applicant's goals for participation of certified M/WBE firms in the design, engineering and construction of the project, and of city residents in the construction work. The City encourages goals of (i) 26 percent MBE and 6 percent WBE participation (measured against the total construction budget for the project or any phase thereof), and (ii) 50 percent city resident hiring (measured against the total construction work hours for the project or any phase thereof). The M/WBE Participation Proposal must include a description of the applicant's proposed outreach plan designed to inform M/WBEs and city residents of job and contracting opportunities. Second, at the time of the applicant's submission for Part II permit review for the project or any phase thereof, the applicant must submit to the Department of Planning and Development: (a) updates (if any) to the applicant's preliminary outreach plan; (b) a description of the applicant's outreach efforts and evidence of such outreach, including, without limitation, copies of certified letters to M/WBE contractor associations and the ward office of the alderman in which the project is located and receipts thereof; (c) responses to the applicant's outreach efforts; and (d) updates (if any) to the applicant's M/WBE and city resident participation goals. Third, prior to issuance of a Certificate of Occupancy for the project or any phase thereof, the applicant must provide the Department of Planning and Development with the actual level of M/WBE and city resident participation in the project or any phase thereof, and evidence of such participation. In addition to the foregoing, the Department of Planning and Development may request such additional information as the Department determines may be necessary or useful in evaluating the extent to which M/WBEs and city residents are informed of and utilized in planned development projects. All such information will be provided in a form acceptable to the Zoning Administrator. The Department of Planning and Development will report the data it collects regarding projected and actual employment of M/WBEs and city residents in planned development projects twice yearly to the Chicago Plan Commission and annually to the Chicago City Council and the Mayor.

16. This Planned Development shall be governed by Section 17-13-0612 of the Chicago Zoning Ordinance. Should this Planned Development lapse, the Zoning Administrator shall initiate a zoning map amendment to rezone the Property to a C2-5 Motor Vehicle-Related Commercial District.
17. Prior to the Part II approval (Section 17-13-0610 of the Chicago Zoning Ordinance) in Subareas C and D, the Applicant shall submit building elevations for the specific subarea(s) for review and approval by the Department of Planning and Development. Future development of a drive-through facility use in Subarea B shall be subject to Site Plan Review. Also, with respect to Site Plan Review for the drive-through facility, Applicant shall submit an updated Traffic Study to the Department of Transportation and Department of Planning and Development. Provided that the required hereunder is in general conformance with the Planned Development and provided Applicant has timely provided all Site Plan Submittals, the Zoning Administrator shall issue such Site Plan Decision. The foregoing notwithstanding, prior to the Zoning Administrator's issuance of such Site Plan Decision, the Applicant shall present the Site Plan (as a courtesy presentation) to the Chicago Plan Commission, during a public meeting. Following approval of the Site Plan for the drive-through facility by the Zoning Administrator, the Site Plan shall be kept on permanent file with the Department of Planning and Development and shall be deemed to be an integral part of this Planned Development. Review and approval by the Department of Planning and Development is intended to assure that specific development components substantially conform with this Planned Development and to assist the City in monitoring ongoing development. Subarea Site Plan Approval Submittals (Section 17-13-0800) need only include that portion of the Property for which approval is being sought by the Applicant. If the Applicant is seeking approval for a portion of the Property that represents less than an entire subarea, the Applicant shall also include a site plan for that area of the Property which is bounded on all sides by either public Rights-of-Way or the boundary of the nearest subarea. The site plan provided shall include all dimensioned and planned street rights-of-way.

No Part II approval for any portion of the Property shall be granted until Site Plan approval has been granted. Following approval by the Department of Planning and Development, the approved Subarea Site Plan Approval Submittals, supporting data and materials shall be made part of the main file and shall be deemed to be an integral part of the P.D.

After approval of the Subarea Site Plan, changes or modifications may be made pursuant to the provisions of Statement 17. In the event of any inconsistency between approved plans and the terms of this Planned Development, the terms of this Planned Development shall govern. Any Subarea Site Plan Approval Submittals shall, at a minimum, provide the following information:

- fully-dimensioned site plan (including a footprint of the proposed improvements);
- fully-dimensioned building elevations;

- fully-dimensioned landscape plan(s); and
- statistical information applicable to the subject subarea, including floor area, the applicable floor area ratio, uses to be established, building heights and setbacks.

Subarea Site Plan Approval Submittals shall include all other information necessary to illustrate substantial conformance to this Planned Development.

[Existing Zoning Map; Existing Land-Use Map; Aerial Map; Boundary Map; Existing and Proposed Right-of-Way Plan; Typical Row Section; Rendered Site Plan; Rendered Site Plan -- Phase 1; Subarea Map; Subarea Plan; Phasing Plan; Rendered Landscape Plan; Rendered Landscape Plan -- Phase 1; Rendered Green Roof Plan; Rendered Green Roof Plan -- Phase 1; North, South, East and West Building Elevations -- Phase 1; Hospital Section + Streetscape; Hospital Bridge; Fitness; Retail North + School + Streetscape; Retail South + Surgery Center + Streetscape; and Garage South referred to in these Plan of Development Statements printed on pages 44028 through 44052 of this *Journal*.]

Bulk Regulations and Data Table referred to in these Plan of Development Statements reads as follows:

Amended Institutional-Business Planned Development No. 1212.

Bulk Regulations And Data Table.

Gross Site Area (square feet):	1,352,957
Area of Public Rights-of-Way (square feet): (See Footnote 1)	94,359

Footnote 1: Per the survey, there is 77,223 square feet in the existing right-of-way adjacent to the Property. The Applicant proposes to dedicate 17,136 square feet of the Property to the existing right-of-way resulting in a total Area of Public Rights-of-Way of 94,359 square feet.

Net Site Area (square feet): 1,258,598

Subarea A (square feet): 758,475

Subarea B (square feet): 271,823

Subarea C (square feet): 127,600

Subarea D (square feet): 100,700

Maximum Floor Area Ratio: 2.00

Subarea A: 2.00

Subarea B: 1.50

Subarea C: 1.50

Subarea D: 2.00

Minimum Setbacks:

West 31st Street: 20 feet

South Kedzie Avenue: 20 feet

Southern boundary of property: 50 feet

Western boundary of property: 40 feet

Maximum Building Height:

Subarea A: 240 feet

Subarea B: 85 feet

Subarea C: In accordance with plans

Subarea D: In accordance with plans

Maximum Number of Hospital Beds: 151

Minimum Number of Off-Street Parking
and Loading:

Subarea A:

	Cars	Bikes	Loading
TOTAL:	726	85	9

Subarea B:

	Cars	Bikes	Loading
TOTAL:	369	54	5

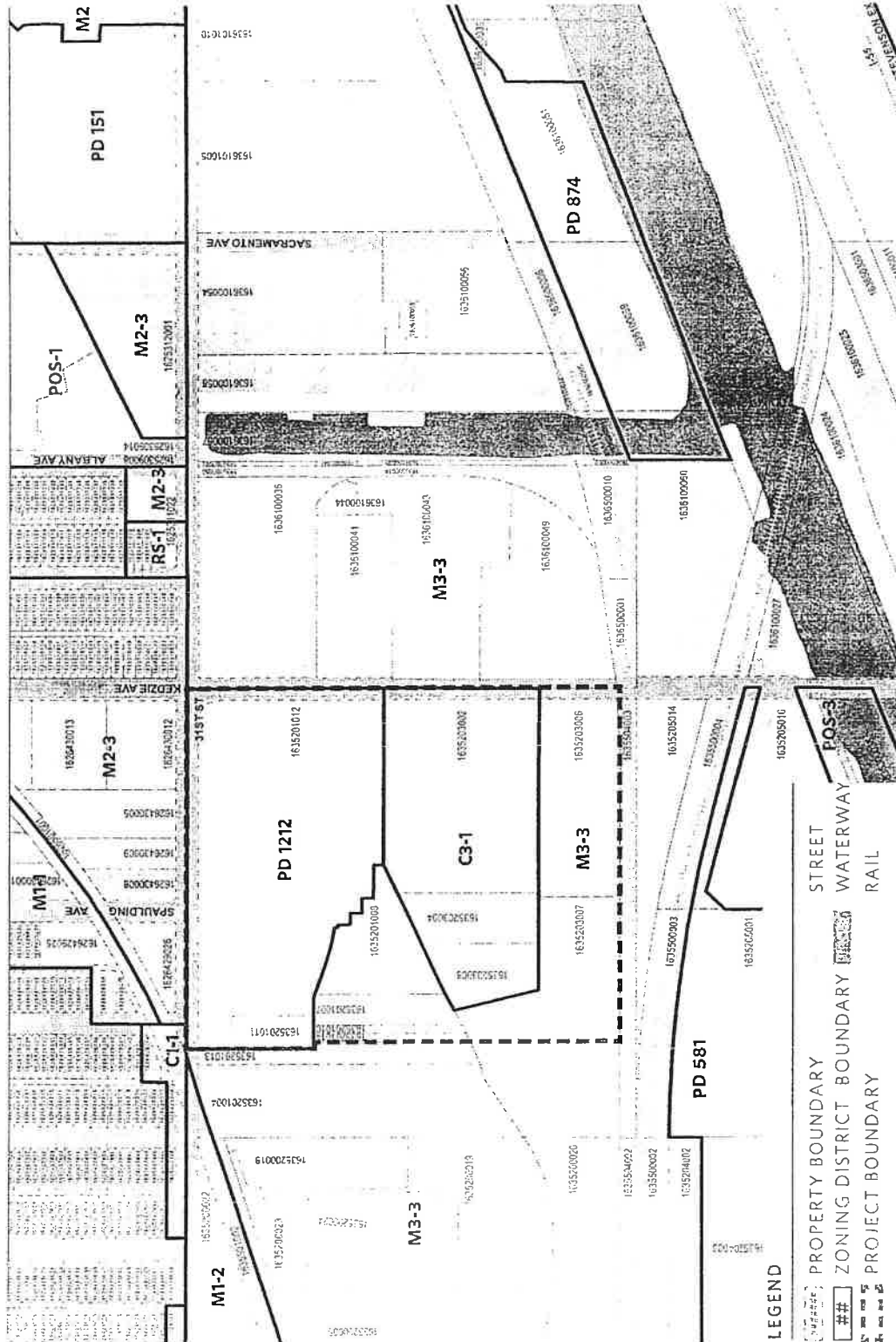
Subarea C:

	Cars	Bikes	Loading
TOTAL:	12	3	0

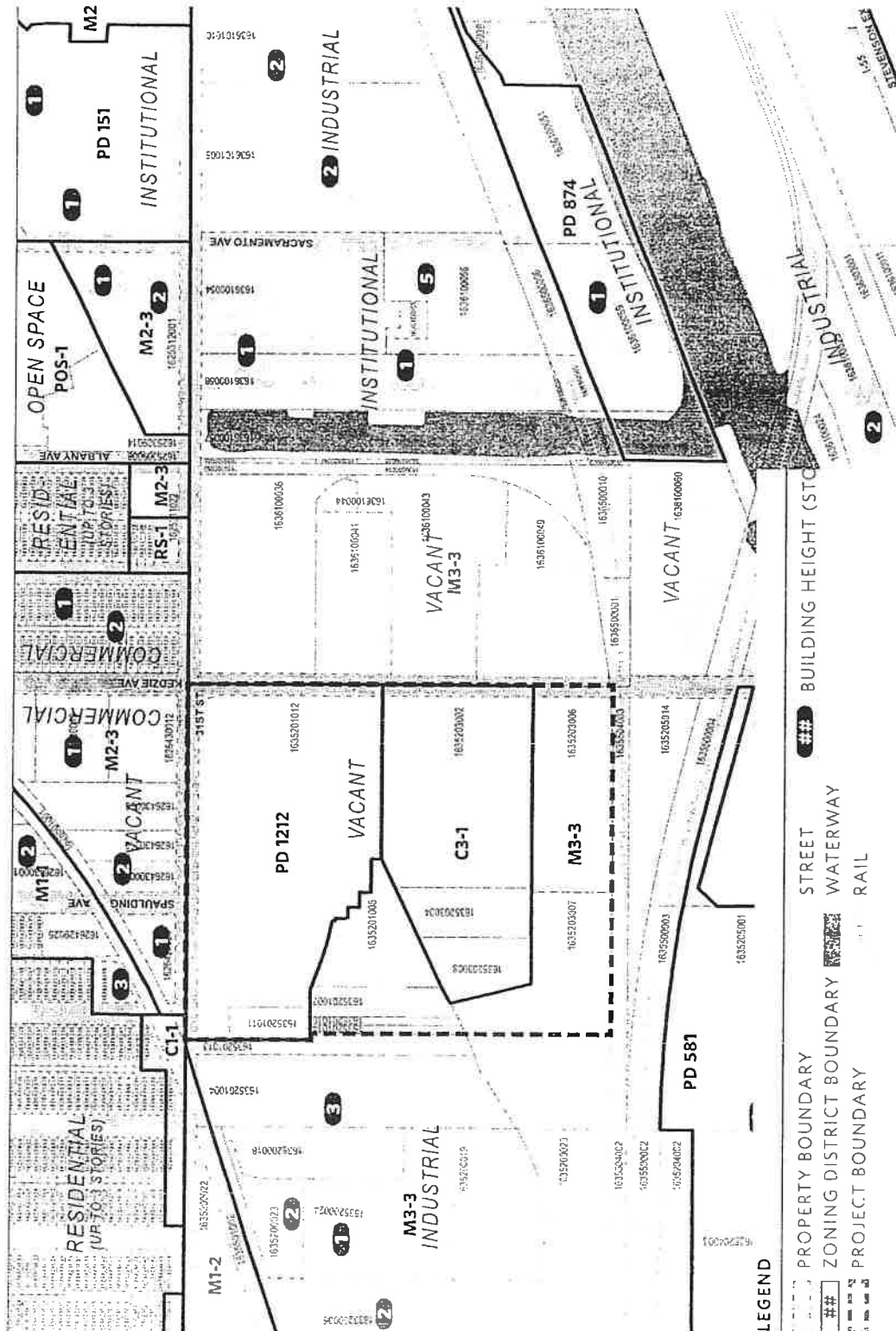
Subarea D:

	Cars	Bikes	Loading
TOTAL:	173	22	3

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FINAL FOR PUBLICATION



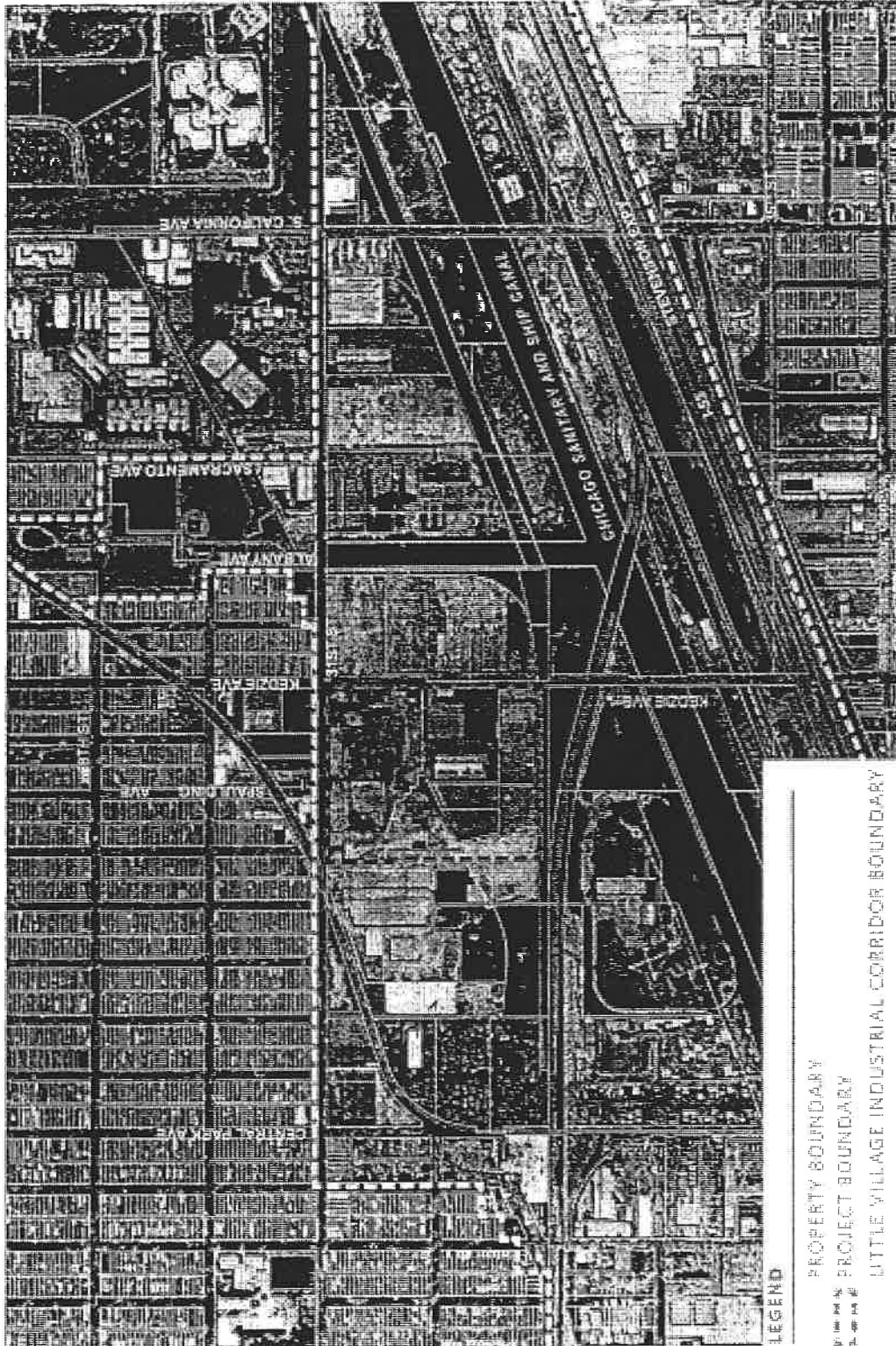
EXISTING LAND-USE MAP

APPLICANT: CHICAGO SOUTHWEST DEVELOPMENT CORPORATION
ADDRESS: 3201-3345 WEST 31ST STREET AND 3100-3258 SOUTH KEDZIE AVENUE
INTRODUCED: NOVEMBER __, 2021
PLAN COMMISSION: _____

30 WEST MONROE
SUITE 700
CHICAGO, IL 60603

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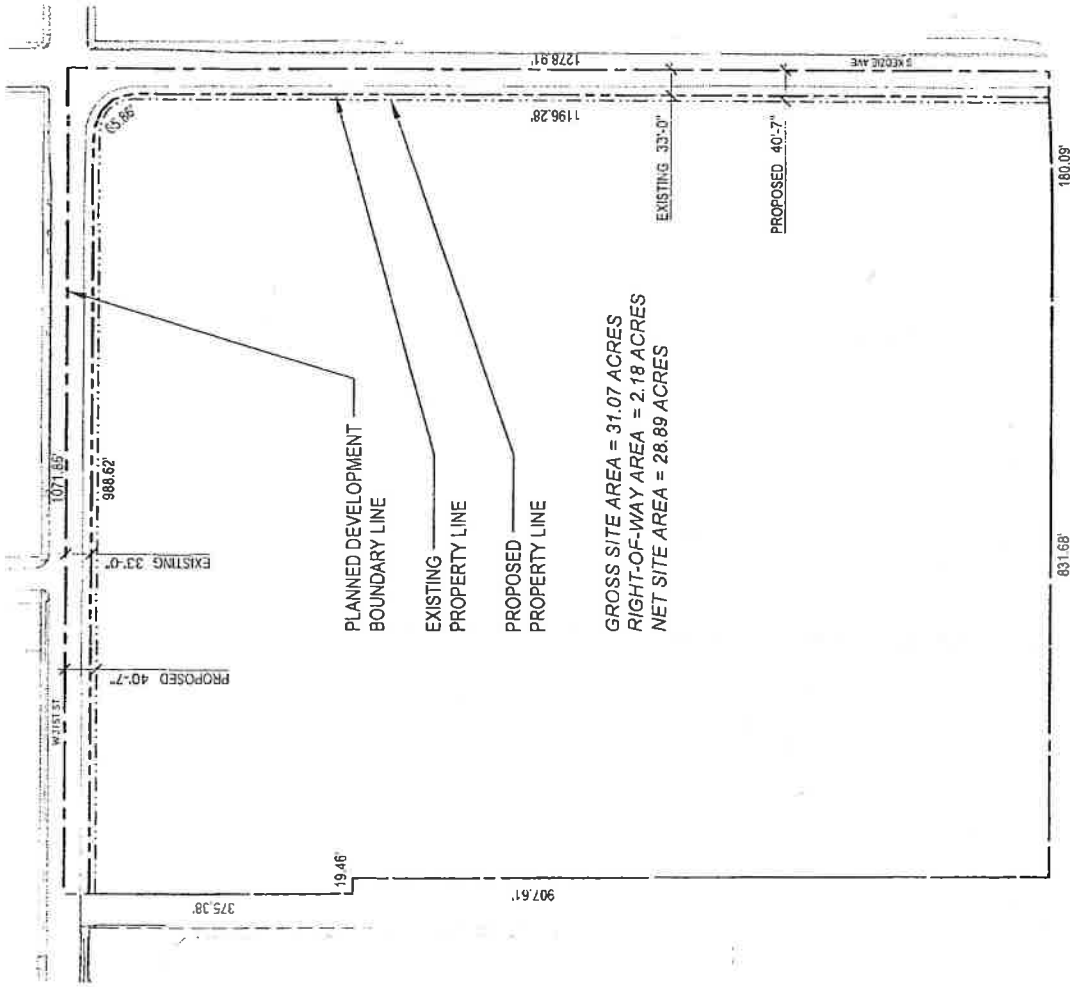


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LEGEND:

- EXISTING PROPERTY LINE
- - - PROPOSED PROPERTY LINE
- PLANNED DEVELOPMENT BOUNDARY LINE



PD BOUNDARY MAP

APPLICANT: CHICAGO SOUTHWEST DEVELOPMENT CORPORATION
 ADDRESS: 3201-3345 WEST 31ST STREET AND 3100-3258 SOUTH KEDZIE AVENUE
 INTRODUCED: NOVEMBER __, 2021
 PLAN COMMISSION: _____

30 WEST MONROE
 SUITE 700
 CHICAGO, IL 60603

400'

200'

100'

0'



0'

100'

200'

400'

800'

1200'

1600'

2000'

2400'

2800'

3200'

3600'

4000'

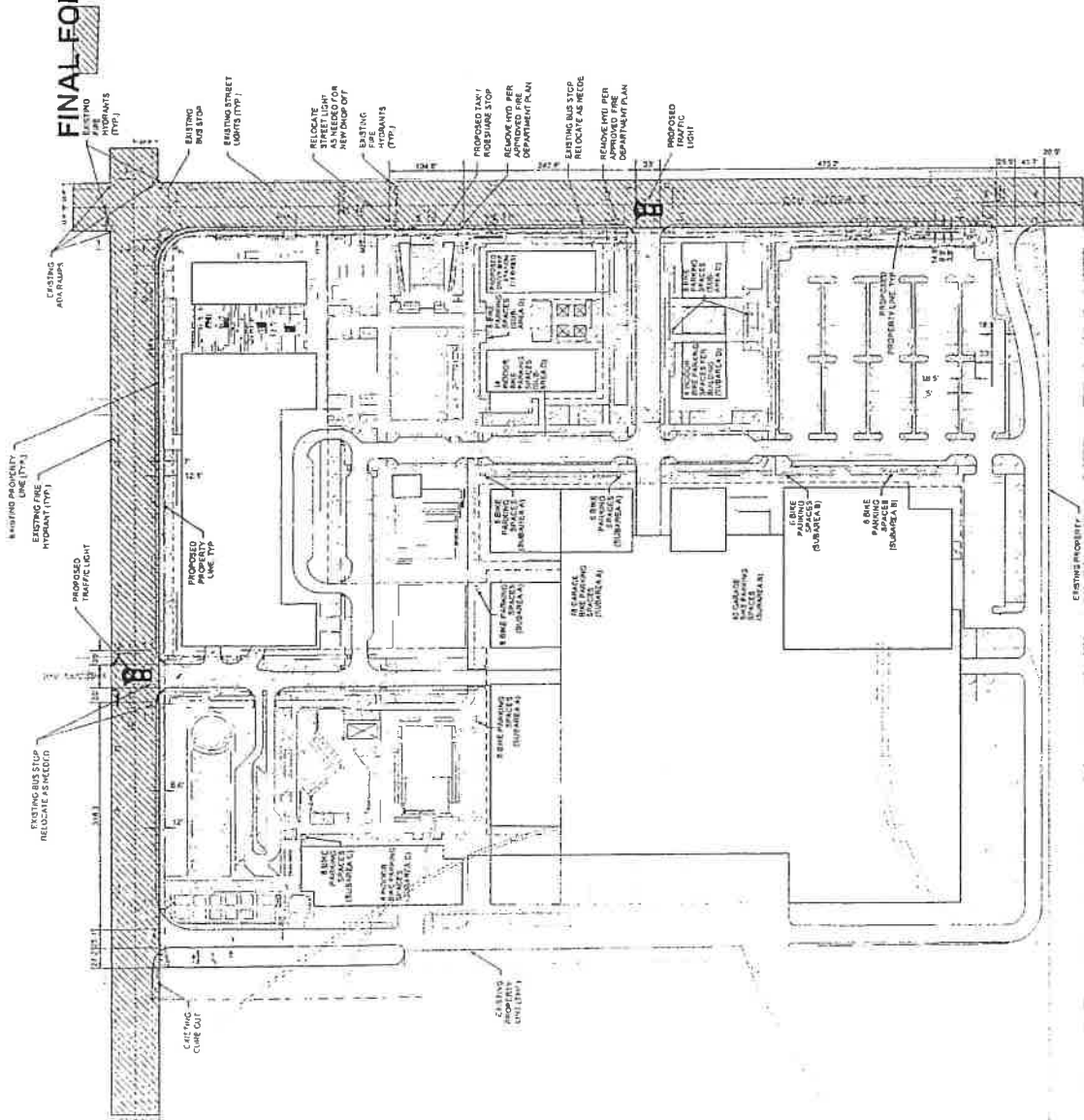
4400'



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EXISTING RIGHT-OF-WAY (ROW)

DEDICATED RIGHT-OF-WAY (ROW)



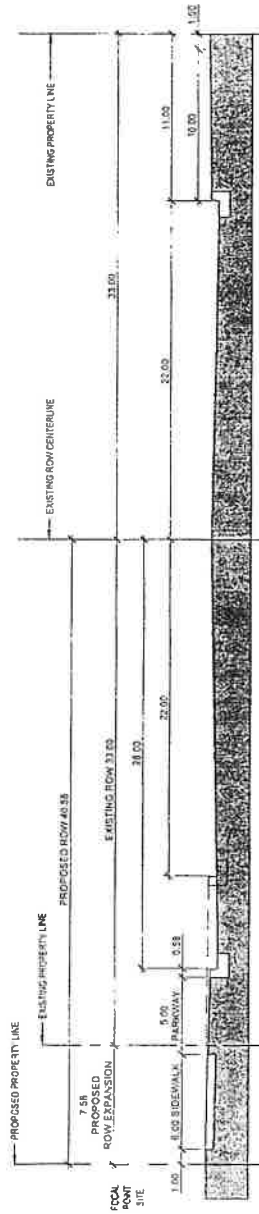
EXISTING AND PROPOSED RIGHT-OF-WAY PLAN

APPLICANT: CHICAGO SOUTHWEST DEVELOPMENT CORPORATION
ADDRESS: 3201-3345 WEST 31ST STREET AND 3100-3258 SOUTH KEDZIE AVENUE
INTRODUCED: NOVEMBER __, 2021
PLAN COMMISSION: _____

20

30 WEST MONROE
SUITE 700
CHICAGO, IL 60603

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TYPICAL ROW SECTION

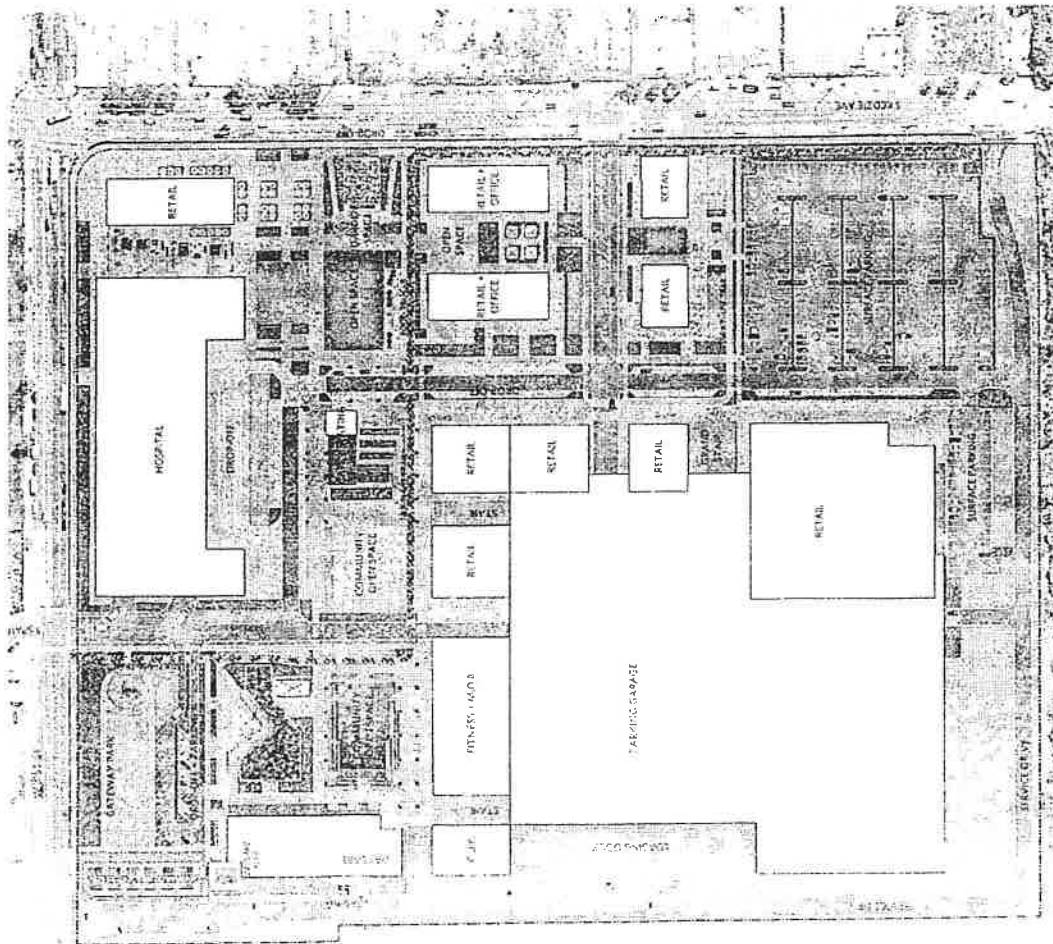
APPLICANT: CHICAGO SOUTHWEST DEVELOPMENT CORPORATION
ADDRESS: 3201-3345 WEST 31ST STREET AND 3100-3258 SOUTH KEDZIE AVENUE
INTRODUCED: NOVEMBER __, 2021
PLAN COMMISSION: _____

30 WEST MONROE
SUITE 700
CHICAGO, IL 60603



0' 5' 10' 20'

FINAL FOR PUBLICATION

**RENDERED SITE PLAN**

APPLICANT: CHICAGO SOUTHWEST DEVELOPMENT CORPORATION
 ADDRESS: 3201-3345 WEST 31ST STREET AND 3100-3258 SOUTH KEDZIE AVENUE
 INTRODUCED: NOVEMBER __, 2021
 PLAN COMMISSION: _____



0'

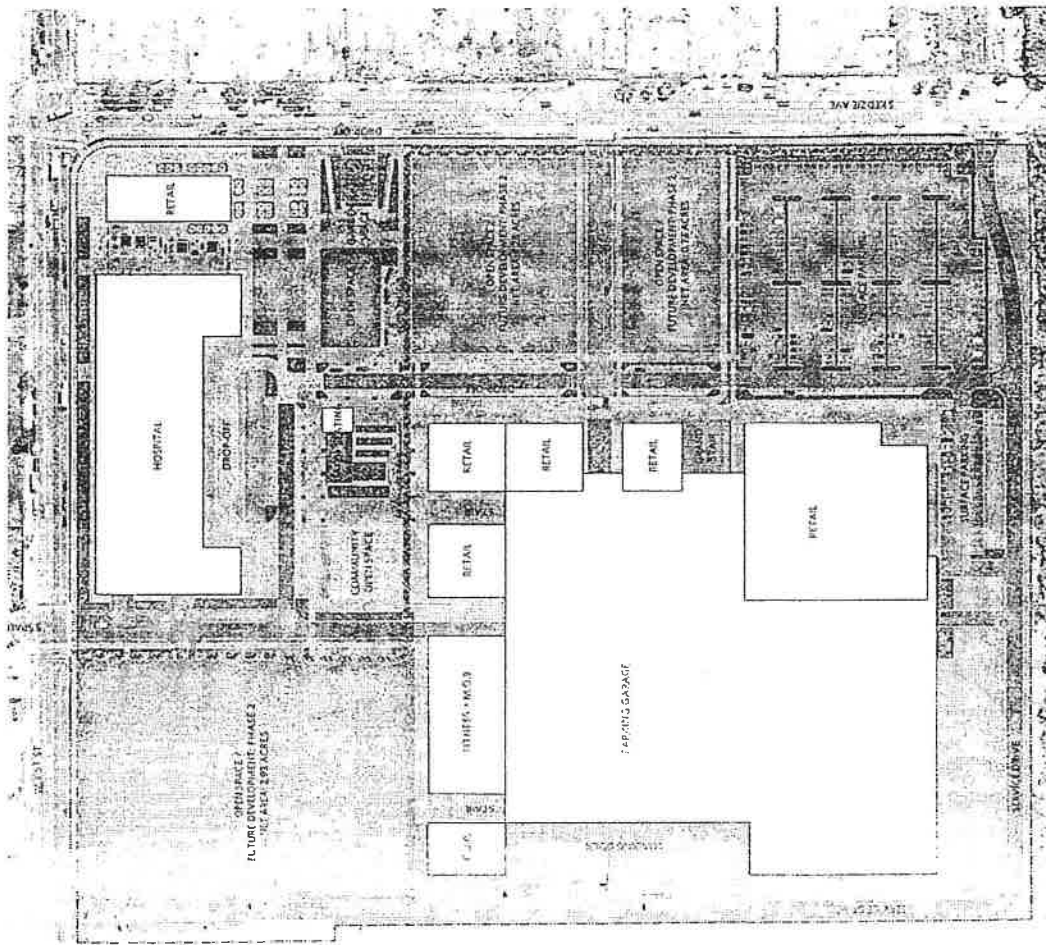
100'

200'

400'

30 WEST MONROE
 SUITE 700
 CHICAGO, IL 60603

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RENDERED SITE PLAN: PHASE 1

APPLICANT: CHICAGO SOUTHWEST DEVELOPMENT CORPORATION
 ADDRESS: 3201-3345 WEST 31ST STREET AND 3100-3258 SOUTH KEDZIE AVENUE
 INTRODUCED: NOVEMBER 2021
 PLAN COMMISSION: _____



30 WEST MONROE
 SUITE 700
 CHICAGO, IL 60603

400'

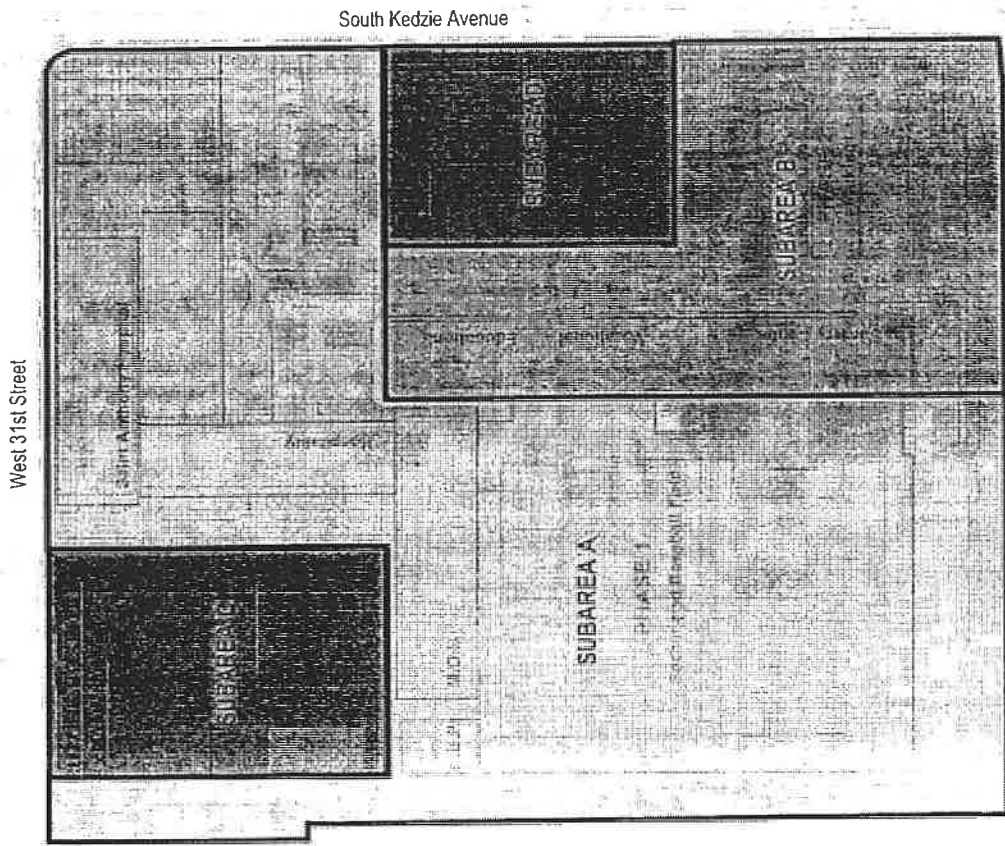
200'

100'

0'



FINAL FOR PUBLICATION



SUBAREA MAP

APPLICANT: CHICAGO SOUTHWEST DEVELOPMENT CORPORATION
 ADDRESS: 3201-3345 WEST 31ST STREET AND 3100-3258 SOUTH KEDZIE AVENUE
 INTRODUCED: NOVEMBER 2021
 PLAN COMMISSION: _____



0'

100'

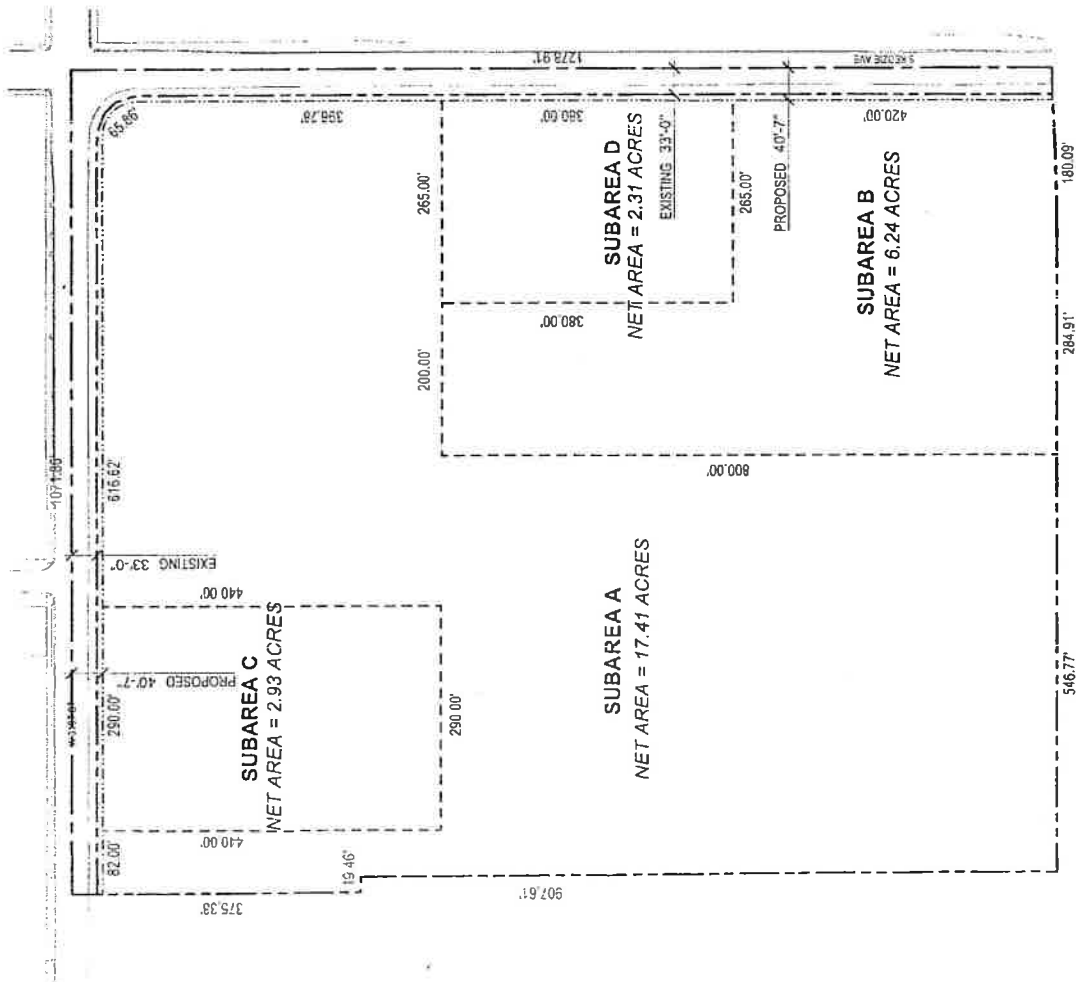
200'

400'



30 WEST MONROE
 SUITE 700
 CHICAGO, IL 60603

FINAL FOR PUBLICATION



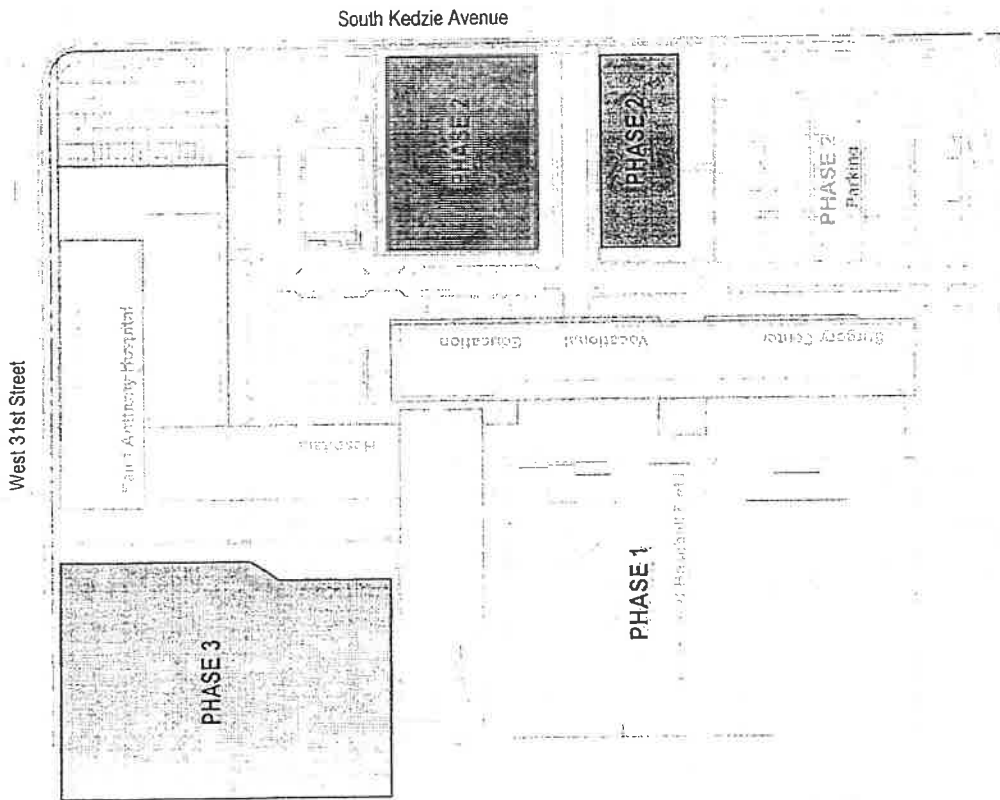
SUBAREA PLAN

APPLICANT: CHICAGO SOUTHWEST DEVELOPMENT CORPORATION
ADDRESS: 3201-3345 WEST 31ST STREET AND 3100-3258 SOUTH KEDZIE AVENUE
INTRODUCED: NOVEMBER __, 2021
PLAN COMMISSION: _____

30 WEST MONROE
SUITE 700
CHICAGO, IL 60603

202

FINAL FOR PUBLICATION

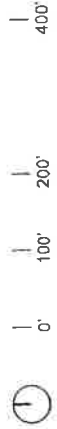


PHASING PLAN

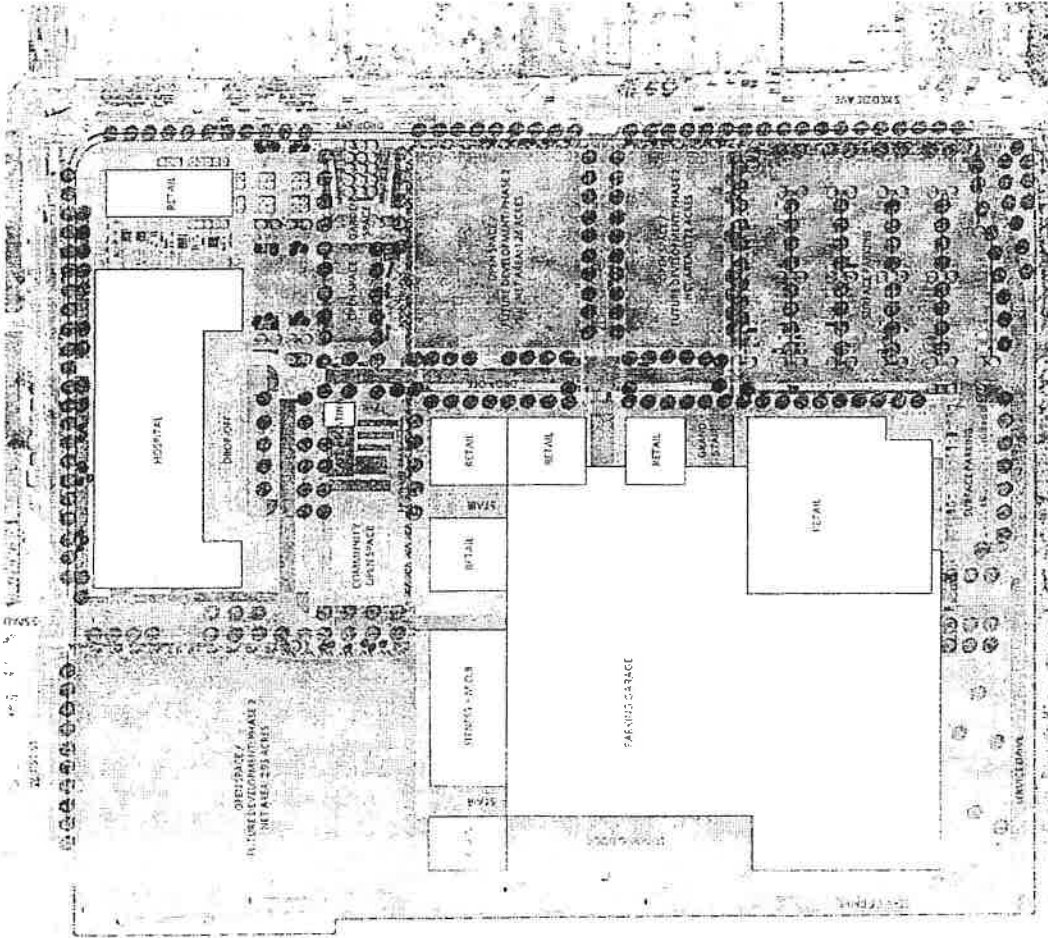
APPLICANT: CHICAGO SOUTHWEST DEVELOPMENT CORPORATION
 ADDRESS: 3201-3345 WEST 31ST STREET AND 3100-3258 SOUTH KEDZIE AVENUE
 INTRODUCED: NOVEMBER 2021
 PLAN COMMISSION: _____



30 WEST MONROE
 SUITE 700
 CHICAGO, IL 60603



FINAL FOR PUBLICATION



Sub-Area C Permitted Uses: Household Living; Dwelling Units located above the ground floor; Household Living; Multi-Unit (3+ units); Residential; Day Care; Parks and Recreation; Community Garden; Eating and Drinking Establishments; Restaurant; Limited; Eating and Drinking Establishments; Restaurant; General; Eating and Drinking Establishments; Outdoor patio (if located on a rooftop); Eating and Drinking Establishments; Outdoor patio (if located at grade level); Entertainment and Spectator Sports; Medium venues (150-999 occupancy); Entertainment and Spectator Sports; Large venues (1,000+ occupancy); Repair or Laundry Service; Consumer Dry cleaning drop-off or pick-up (no on-premise plant); Sports and Recreation; Participant; Children's Play Center; Wireless Communication Facilities; Co-located; and related accessory uses.

Sub-Area D Permitted Uses: Artist Work or Sales Space; Drive-Through Facility; Eating and Drinking Establishments; Restaurant; Limited; Eating and Drinking Establishments; Restaurant; General; Eating and Drinking Establishments; Tavern; Eating and Drinking Establishments; Outdoor patio (if located on a rooftop); Eating and Drinking Establishments; Outdoor patio (if located at grade level); Entertainment and Spectator Sports; Indoor Special Event including incidental liquor sales; Financial Services; Bank; Savings Bank; Savings and Loan Association; Currency Exchange and Credit Union; Financial Services; Automated Teller Machine Facility; Flea Market; Food and Beverage Retail Sales; Liquor Sales (as accessory use); Medical Service; Office but excluding Electronic Data Storage Center; Personal Service; Hair Salon, Nail Salon, or Barbershop; Personal Service; Massage Establishment; Retail Sales; General; Manufacturing, Production and Industrial Services; Artisan; Manufacturing, Production and Industrial Services; Limited (catering & shared kitchen only); Manufacturing, Production and Industrial Services; Limited; Wireless Communication Facilities; Co-located; Cannabis Business Establishments; Medical Cannabis Dispensary; and related accessory uses.

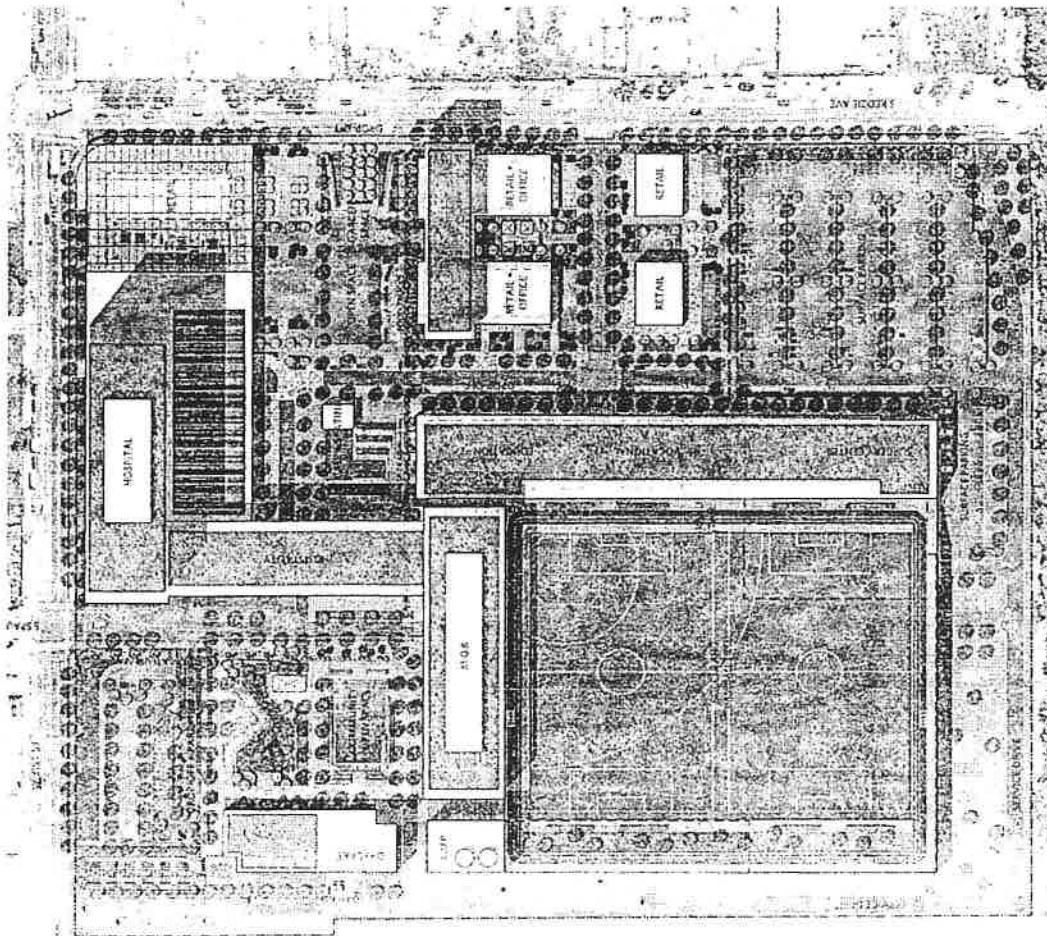
RENDERED LANDSCAPE PLAN: PHASE 1

APPLICANT: CHICAGO SOUTHWEST DEVELOPMENT CORPORATION
ADDRESS: 3201-3345 WEST 31ST STREET AND 3100-3258 SOUTH KEDZIE AVENUE
INTRODUCED NOVEMBER 2021
PLAN COMMISSION:

30 WEST MONROE
SUITE 700
CHICAGO, IL 60603



FINAL FOR PUBLICATION



RENDERED GREEN ROOF PLAN

APPLICANT: CHICAGO SOUTHWEST DEVELOPMENT CORPORATION
 ADDRESS: 3201-3345 WEST 31ST STREET AND 3100-3258 SOUTH KEDZIE AVENUE
 INTRODUCED: NOVEMBER __, 2021
 PLAN COMMISSION: _____



30 WEST MONROE
 SUITE 700
 CHICAGO, IL 60603

400'

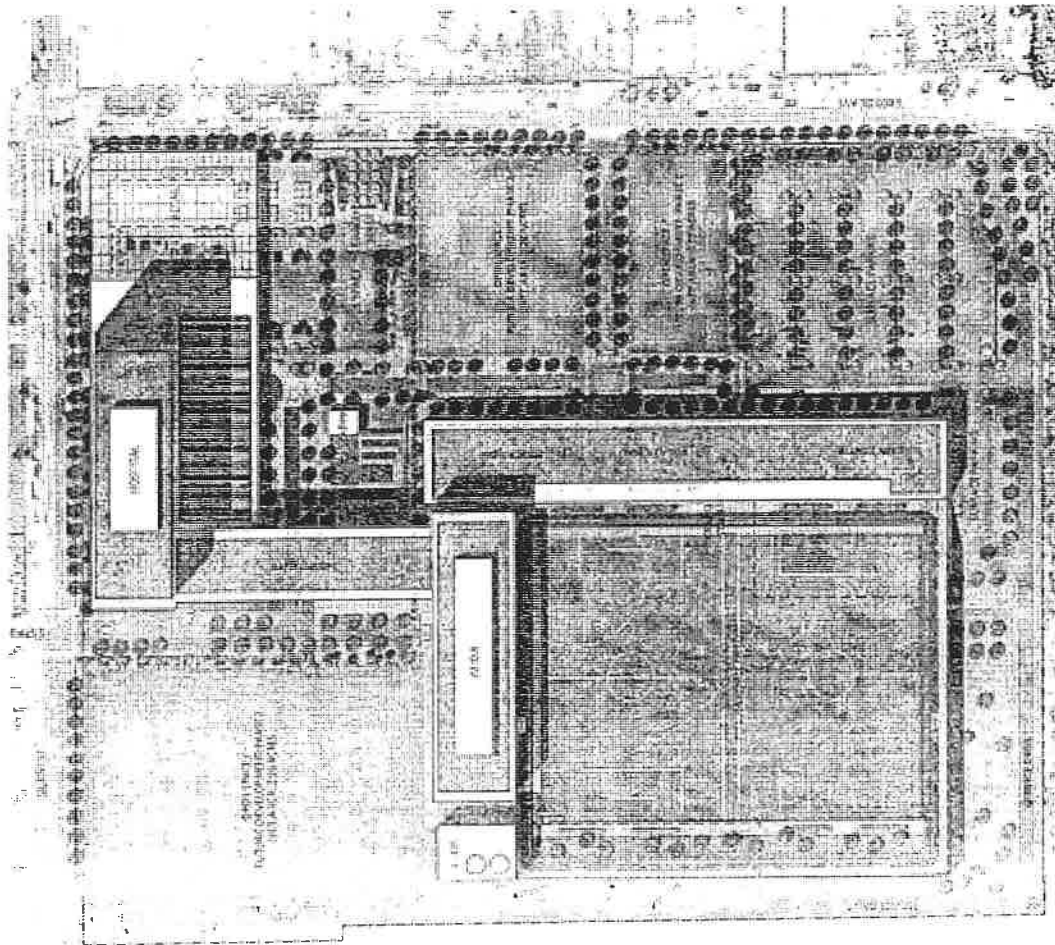
200'

100'

0'



FINAL FOR PUBLICATION



RENDERED GREEN ROOF PLAN: PHASE 1

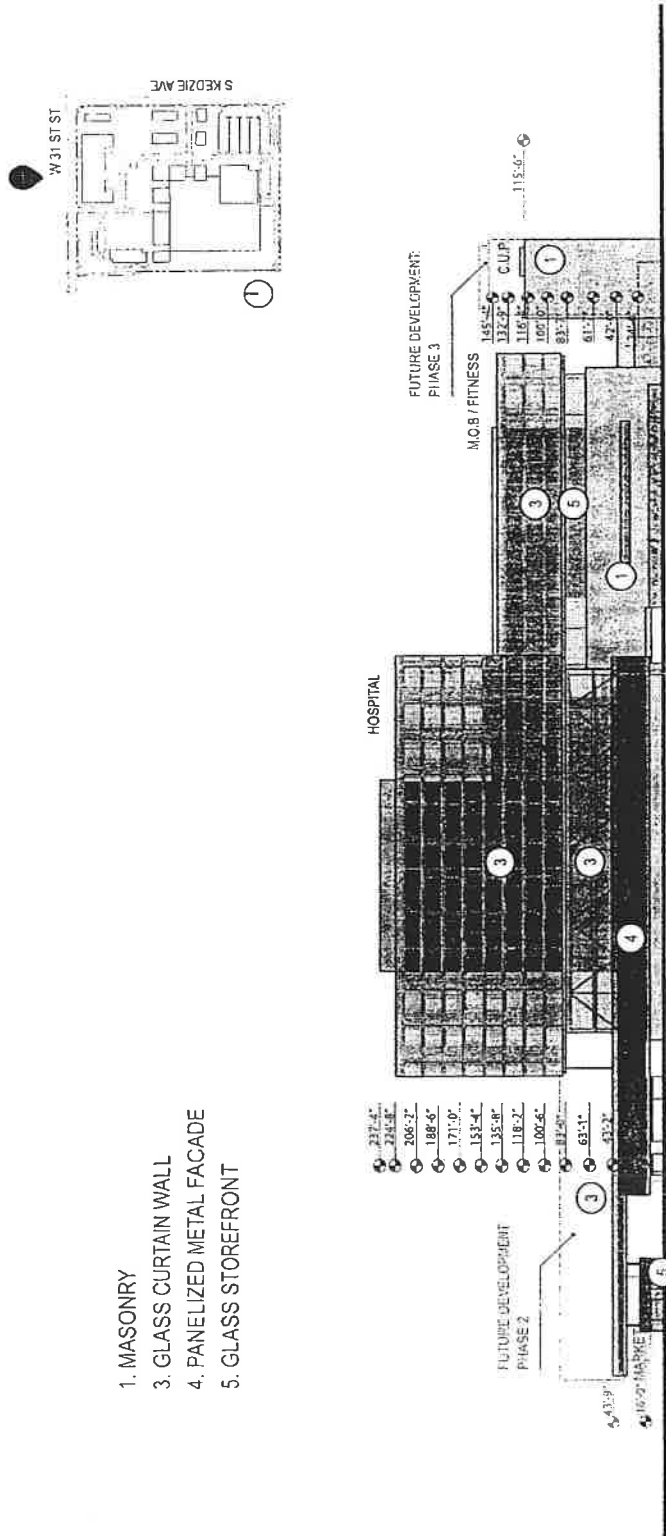
APPLICANT: CHICAGO SOUTHWEST DEVELOPMENT CORPORATION
 ADDRESS: 3201-3345 WEST 31ST STREET AND 3100-3258 SOUTH KEDZIE AVENUE
 INTRODUCED: NOVEMBER 2021
 PLAN COMMISSION

FOR

30 WEST MONROE
 SUITE 700
 CHICAGO, IL 60603

0' 100' 200' 400'

FINAL FOR PUBLICATION



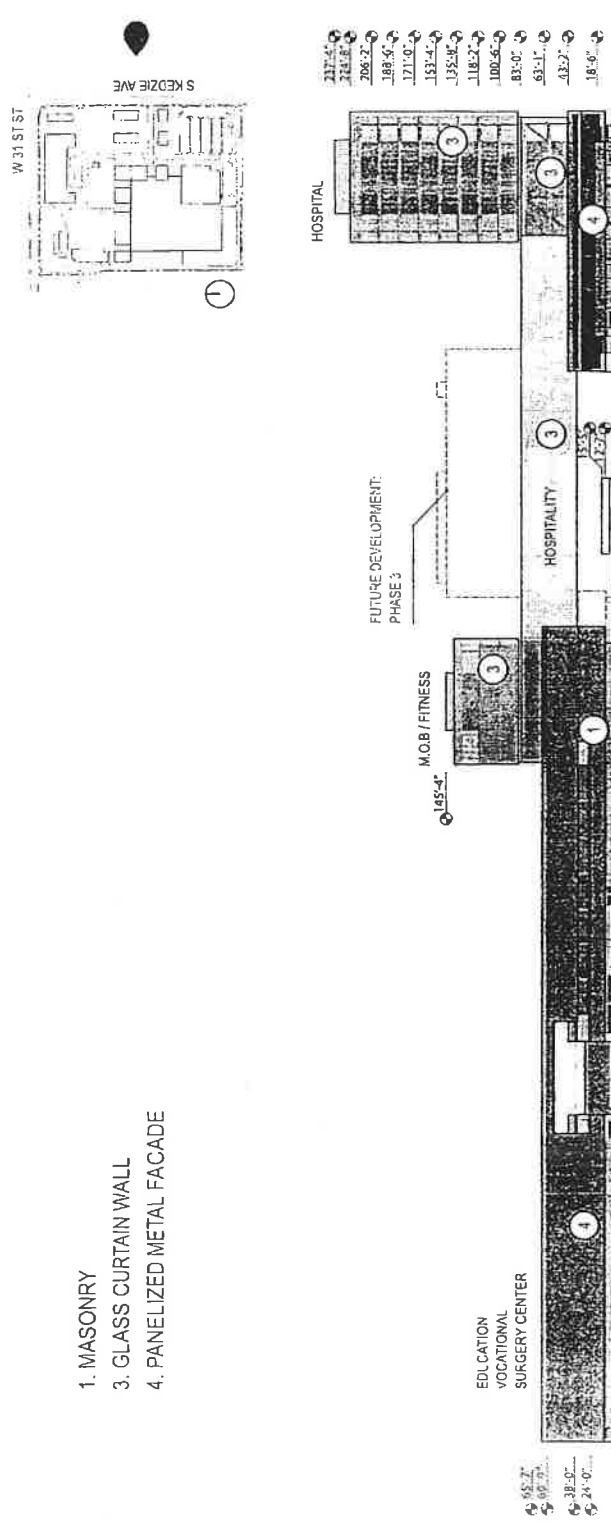
NORTH ELEVATION: PHASE 1

APPLICANT CHICAGO SOUTHWEST DEVELOPMENT CORPORATION
ADDRESS: 3201-3345 WEST 31ST STREET AND 3100-3258 SOUTH KEDZIE AVENUE
INTRODUCED: NOVEMBER __, 2021
PLAN COMMISSION: _____

30 WEST MONROE
SUITE 700
CHICAGO, IL 60603

20

FINAL FOR PUBLICATION



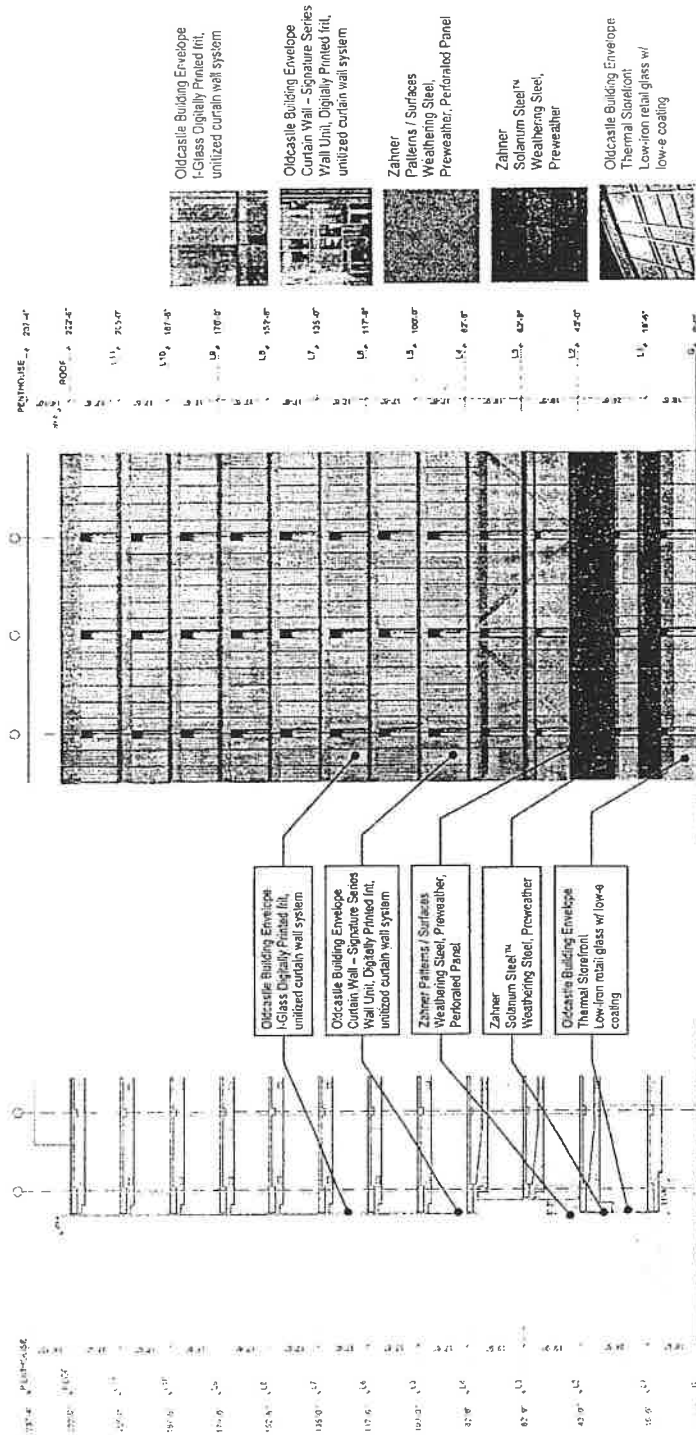
EAST ELEVATION: PHASE 1

APPLICANT: CHICAGO SOUTHWEST DEVELOPMENT CORPORATION
ADDRESS: 3201-3345 WEST 31ST STREET AND 3100-3258 SOUTH KEDZIE AVENUE
INTRODUCED NOVEMBER __, 2021
PLAN COMMISSION: _____

30 WEST MONROE
SUITE 700
CHICAGO, IL 60603

FD

FINAL FOR PUBLICATION



CTC

30 WEST MONROE
SUITE 700
CHICAGO, IL 60603

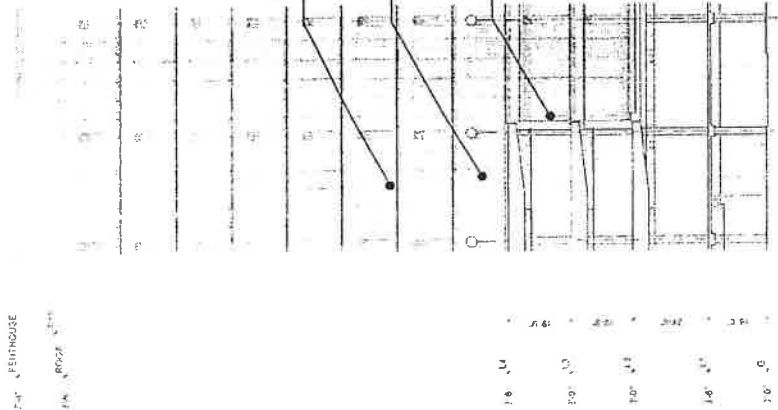
HOSPITAL SECTION + STREETSCAPE

APPLICANT: CHICAGO SOUTHWEST DEVELOPMENT CORPORATION
ADDRESS: 3201-3345 WEST 31ST STREET AND 3100-3258 SOUTH KEDZIE AVENUE
INTRODUCED NOVEMBER __, 2021
PLAN COMMISSION: _____

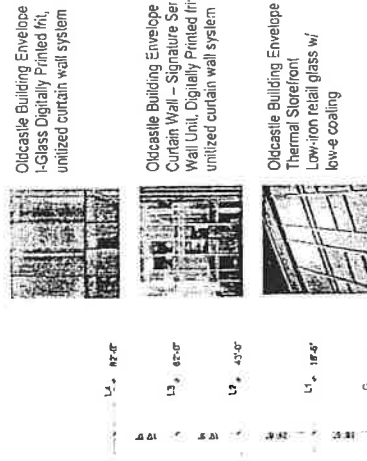
FINAL FOR PUBLICATION

7'-0" FENTHOUSE

1'-0" 80'-0" 2'-0"



SECTION



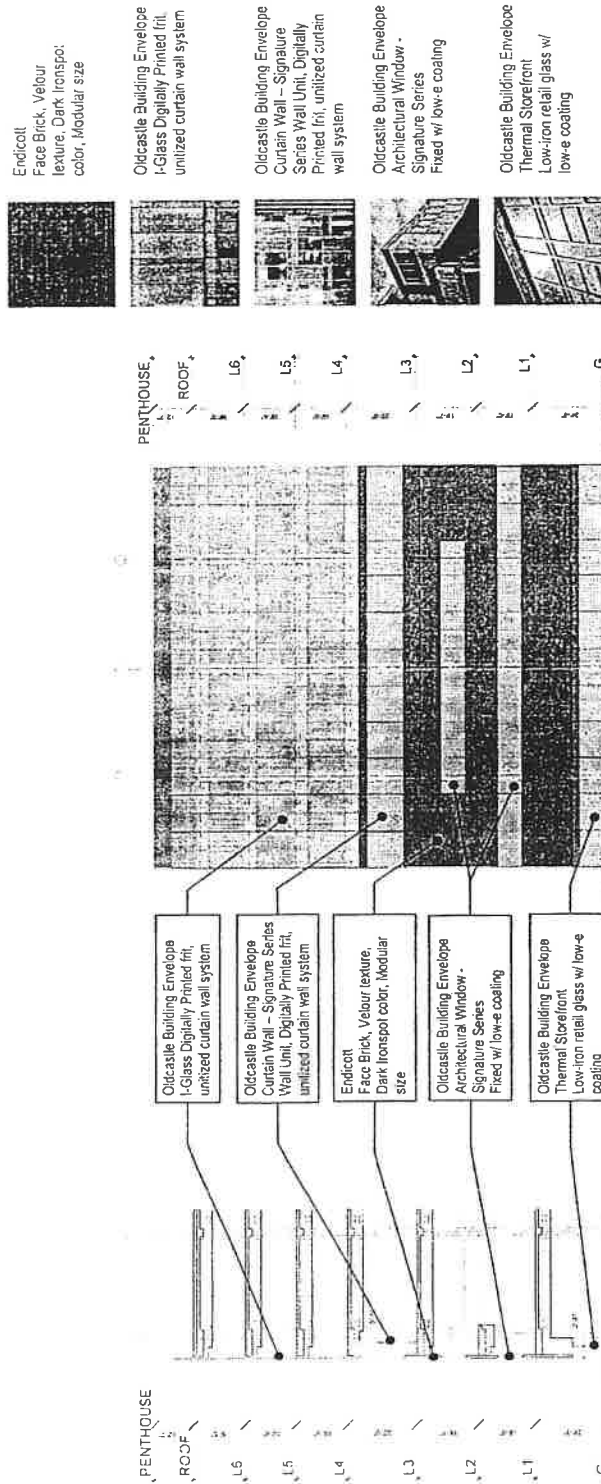
ELEVATION



HOSPITAL BRIDGE
 APPLICANT: CHICAGO SOUTHWEST DEVELOPMENT CORPORATION
 ADDRESS: 3201-3345 WEST 31ST STREET AND 3100-3268 SOUTH KEDZIE AVENUE
 INTRODUCED NOVEMBER 2021
 PLAN COMMISSION

HDR
 30 WEST MONROE
 SUITE 700
 CHICAGO, IL 60603

FINAL FOR PUBLICATION



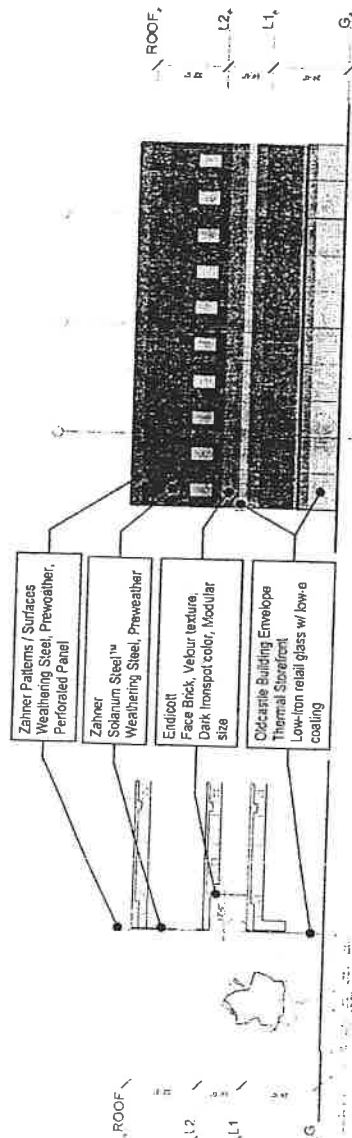
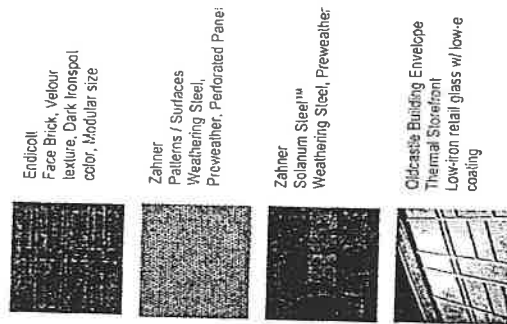
ELEVATION

SECTION

FITNESS
APPLICANT: CHICAGO SOUTHWEST DEVELOPMENT CORPORATION
ADDRESS: 3201-3345 WEST 31ST STREET AND 3100-3258 SOUTH KEDZIE AVENUE
INTRODUCED NOVEMBER 2021
PLAN COMMISSION

HDR
30 WEST MONROE
SUITE 700
CHICAGO, IL 60603

FINAL FOR PUBLICATION



RETAIL NORTH + SCHOOL + STREETSCAPE

APPLICANT: CHICAGO SOUTHWEST DEVELOPMENT CORPORATION
 ADDRESS: 3201-3345 WEST 31ST STREET AND 3100-3268 SOUTH KEDZIE AVENUE
 INTRODUCED: NOVEMBER 2021
 PLAN COMMISSION: _____

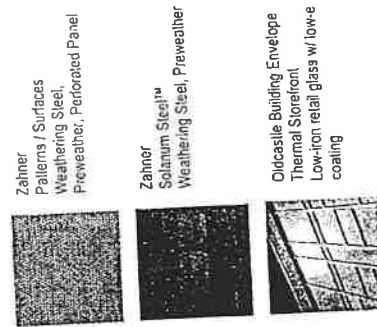
HDR
 30 WEST MONROE
 SUITE 700
 CHICAGO, IL 60603

1/26/2022

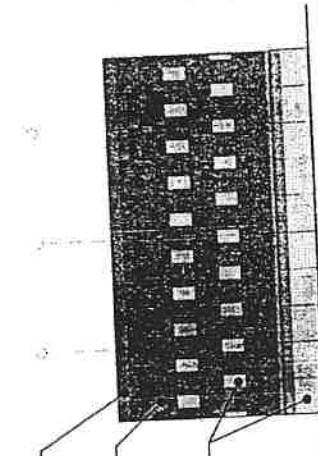
REPORTS OF COMMITTEES

44051

FINAL FOR PUBLICATION



ROOF
L2
L1
G



ELEVATION

ROOF
L2
L1
G

SECTION



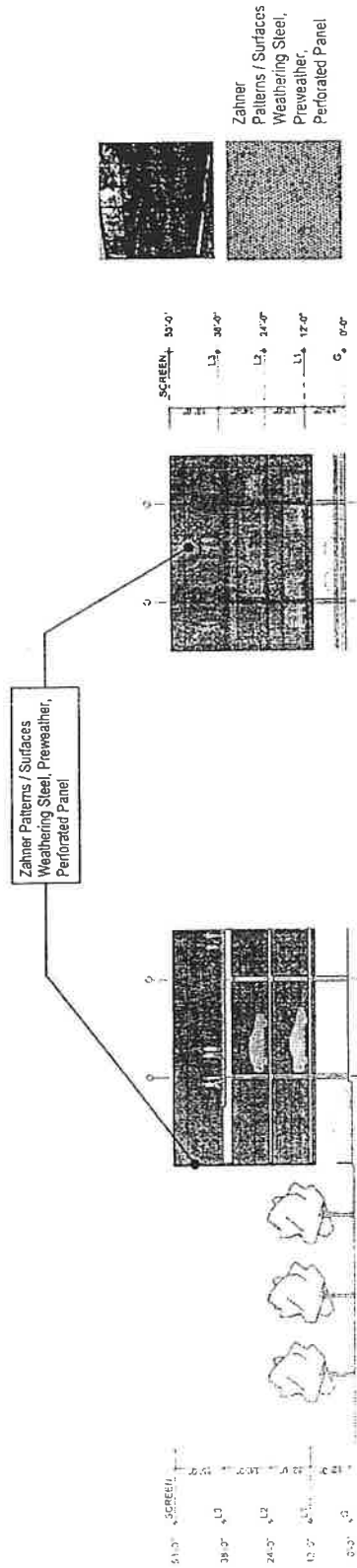
FOR

30 WEST MONROE
SUITE 700
CHICAGO, IL 60603

RETAIL SOUTH + SURGERY CENTER + STREETSCAPE

APPLICANT: CHICAGO SOUTHWEST DEVELOPMENT CORPORATION
ADDRESS: 3201-3345 WEST 31ST STREET AND 3100-3258 SOUTH KEDZIE AVENUE
INTRODUCED NOVEMBER 2021
PLAN COMMISSION:

FINAL FOR PUBLICATION



ELEVATION

SECTION

GARAGE SOUTH
 APPLICANT: CHICAGO SOUTHWEST DEVELOPMENT CORPORATION
 ADDRESS: 3201-3345 WEST 31ST STREET AND 3100-3258 SOUTH KEDZIE AVENUE
 INTRODUCED: NOVEMBER 2021
 PLAN COMMISSION: _____

HR

30 WEST MONROE
 SUITE 700
 CHICAGO, IL 60603

17550

3/13/2013

REPORTS OF COMMITTEES

49227

Reclassification Of Area Shown On Map No. 7-J.
(Application No. 17658)
(Common Address: 2933 N. Avers Ave.)

[O2013-38]

~~Be It Ordained by the City Council of the City of Chicago:~~

~~SECTION 1. Title 17 of the Municipal Code of Chicago, the Chicago Zoning Ordinance, is hereby amended by changing all of the RS3 Residential Single-Unit (Detached) District symbols and indications as shown on Map Number 7-J in the area bounded by:~~

~~a line that is 270 feet south of West Wellington Avenue; the public alley east of and parallel to North Avers Avenue; a line that is 297 feet south of West Wellington Avenue; and North Avers Avenue,~~

~~to those of an RT4 Residential Two-Flat, Townhouse or Multi-Unit District.~~

~~SECTION 2. This ordinance takes effect after its passage and approval.~~

IBPD 1212

Reclassification Of Area Shown On Map No. 8-J.
(As Amended)
(Application No. 17550)

(Common Address: 3201 -- 3345 W. 31st St. And 3100 -- 3150 S. Kedzie Ave.)

[SO2012-4962]

Be It Ordained by the City Council of the City of Chicago:

SECTION 1. That the Chicago Zoning Ordinance be amended by changing all the current M3-3 Heavy Industry District symbols and indications as shown on Map Number 8-J in the area bounded by:

West 31st Street; South Kedzie Avenue; a line 553.59 feet south of and parallel to West 31st Street; a line 495.12 feet west of and parallel to South Kedzie Avenue; a line 523.59 feet south of and parallel to West 31st Street; a line 594.97 feet west of and parallel to South Kedzie Avenue; a line 488.70 feet south of and parallel to West 31st Street; a line 638.85 feet west of and parallel to South Kedzie Avenue; a line 453.33 feet south of and parallel to West 31st Street; a line 671.00 feet west of and parallel to South Kedzie Avenue; a line from a point 671.00 feet west of South Kedzie Avenue and 453.33 feet south of West 31st Street running for a distance of 10.08 feet to a point 672.28 feet west of South Kedzie Avenue and 443.33 feet south of West 31st Street; a line from a point 672.28 feet west of South Kedzie Avenue and 443.33 feet south of West 31st Street running for a distance of 29.27 feet to a point 672.90 feet west of South Kedzie Avenue and 414.07 feet south of West 31st Street; a line 677.00 feet west of South Kedzie Avenue; a line from a point 677.00 feet west of South Kedzie Avenue and

414.07 feet south of West 31st Street running for a distance of 9.80 feet to a point 678.87 feet west of South Kedzie Avenue and 404.27 feet south of West 31st Street; a line from a point 678.87 feet west of South Kedzie Avenue and 404.27 feet south of West 31st Street running for a distance of 109.69 feet to a point 785.08 feet west of South Kedzie Avenue and 377.12 feet south of West 31st Street; a line from a point 785.08 feet west of South Kedzie Avenue and 377.12 feet south of West 31st Street running for a distance of 106.34 to a point 886.08 feet west of South Kedzie Avenue and 342.33 feet south of West 31st Street; a line 342.33 feet south of West 31st Street; a line 880.06 feet west of South Kedzie Avenue; a line 100 feet south of West 31st Street; and a line 977.48 feet west of South Kedzie Avenue,

to those of a C3-3 Commercial, Manufacturing and Employment District which is hereby established in the area described above.

SECTION 2. That the Chicago Zoning Ordinance be amended by changing all the current C3-3 Commercial, Manufacturing and Employment District symbols and indications as shown on Map No. 8-J in the area bounded by:

West 31st Street; South Kedzie Avenue; a line 553.59 feet south of and parallel to West 31st Street; a line 495.12 feet west of and parallel to South Kedzie Avenue; a line 523.59 feet south of and parallel to West 31st Street; a line 594.97 feet west of and parallel to South Kedzie Avenue; a line 488.70 feet south of and parallel to West 31st Street; a line 638.85 feet west of and parallel to South Kedzie Avenue; a line 453.33 feet south of and parallel to West 31st Street; a line 671.00 feet west of and parallel to South Kedzie Avenue; a line from a point 671.00 feet west of South Kedzie Avenue and 453.33 feet south of West 31st Street running for a distance of 10.08 feet to a point 672.28 feet west of South Kedzie Avenue and 443.33 feet south of West 31st Street; a line from a point 672.28 feet west of South Kedzie Avenue and 443.33 feet south of West 31st Street running for a distance of 29.27 feet to a point 672.90 feet west of South Kedzie Avenue and 414.07 feet south of West 31st Street; a line 677.00 feet west of South Kedzie Avenue; a line from a point 677.00 feet west of South Kedzie Avenue and 414.07 feet south of West 31st Street running for a distance of 9.80 feet to a point 678.87 feet west of South Kedzie Avenue and 404.27 feet south of West 31st Street; a line from a point 678.87 feet west of South Kedzie Avenue and 404.27 feet south of West 31st Street running for a distance of 109.69 feet to a point 785.08 feet west of South Kedzie Avenue and 377.12 feet south of West 31st Street; a line from a point 785.08 feet west of South Kedzie Avenue and 377.12 feet south of West 31st Street running for a distance of 106.34 to a point 886.08 feet west of South Kedzie Avenue and 342.33 feet south of West 31st Street; a line 342.33 feet south of West 31st Street; a line 880.06 feet west of South Kedzie Avenue; a line 100 feet south of West 31st Street; and a line 977.48 feet west of South Kedzie Avenue,

to those of an Institutional Business Planned Development which is hereby established in the area described above.

SECTION 3. This ordinance shall be in force and effect from and after its passage and due publication.

Planned Development Statements referred to in this ordinance read as follows:

Institutional-Business Planned Development Statements.

1. The area delineated herein as Planned Development Number 1212 ("Planned Development") consists of approximately 470,812 square feet of property which is depicted on the attached Planned Development Boundary and Property Line Map ("Property") and is owned or controlled by the applicant, Chicago Southwest Development Corporation.
2. The requirements, obligations and conditions contained within this Planned Development shall be binding upon the applicant, its successors and assigns and, if different than the applicant, the legal titleholders and any ground lessors. All rights granted hereunder to the applicant shall inure to the benefit of the applicant's successors and assigns and, if different than the applicant, the legal titleholder and any ground lessors. Furthermore, pursuant to the requirements of Section 17-8-0400 of the Chicago Zoning Ordinance, the Property, at the time of application for amendments, modifications or changes (administrative, legislative or otherwise) to this Planned Development are made, shall be under single ownership or designated control. Single designated control is defined in Section 17-8-0400 of the Zoning Ordinance.
3. All applicable official reviews, approvals or permits are required to be obtained by the applicant or its successors, assignees or grantees. Any dedication or vacation of streets or alleys or grants of easements or any adjustment of the right-of-way shall require a separate submittal to the Department of Transportation on behalf of the applicant or its successors, assign or grantees.

Any requests for grants of privilege, or any items encroaching on the public way, shall be in compliance with the Plans.

Ingress or egress shall be pursuant to the Plans and may be subject to the review and approval of the Departments of Housing and Economic Development and Transportation. Closure of all or any public street or alley during demolition or construction shall be subject to the review and approval of the Department of Transportation.

All work proposed in the public way must be designed and constructed in accordance with the Department of Transportation Construction Standards for Work in the Public Way and in compliance with the Municipal Code of the City of Chicago. Prior to the issuance of any Part II Approval, the submitted plans must be approved by the Department of Transportation.

The applicant understands that improvements within the public right-of-way may be identified by the traffic impact study submitted to the Department of Transportation. Such improvements may include but are not limited to new traffic signal equipment, left turn arrow signals, countdown pedestrian signals, pavement markings, and adjustments to the location of the existing curb line. All such improvements required by CDOT must be constructed and operational prior to the granting of the Certificate of Occupancy.

4. This Plan of Development consists of sixteen (16) statements: a Bulk Regulations Table; an Existing Land-Use and Zoning Map; a Planned Development Boundary and Property Line Map; a Site Plan; Landscape Plan; Green Roof Plan; and Building Elevations prepared by HDR Architects dated January 24, 2013 submitted herein. Full-sized copies of the Site Plan, Landscape Plan and Building Elevations are on file with the Department of Housing and Economic Development. In any instance where a provision of this Planned Development conflicts with the Chicago Building Code, the Building Code shall control. This Planned Development conforms to the intent and purpose of the Zoning Ordinance, and all requirements thereto, and satisfies the established criteria for approval as a Planned Development. In case of a conflict between the terms of this Planned Development Ordinance and the Zoning Ordinance, this Planned Development Ordinance shall control.
5. The following uses are permitted in the area delineated herein as an Institutional Business Planned Development: Day Care; Hospital; Community Centers, Recreation Buildings and Similar Assembly Uses; Schools; Business Equipment Sale and Service; Business Support Services; Communication Service Establishments; Eating and Drinking Establishments, including at grade or roof top outdoor patios; Indoor Special Event Space; Banquet or Meeting Halls; Banks and other Financial Institutions; ATM Facilities; Food and Beverage Retail Sales; Medical Service; Office; Personal Service; Consumer; Retail Sales, General; Outdoor and Indoor Participant Sports and Recreation; Children's Activity Facility; Wireless Communications Facilities (except free-standing towers); and accessory uses.
6. On-premises signs and temporary signs, such as construction and marketing signs, shall be permitted within the Planned Development, subject to the review and approval of the Department of Housing and Economic Development. Off-premises signs are prohibited within the boundary of the Planned Development.
7. For purposes of height measurement, the definitions in the Zoning Ordinance shall apply. The height of any building shall also be subject to height limitations, if any, established by the Federal Aviation Administration.
8. The maximum permitted Floor Area Ratio ("FAR") for the site shall be in accordance with the attached Bulk Regulations Table. For the purposes of FAR calculations and measurements, the definitions in the Zoning Ordinance shall apply. The permitted FAR identified in the Bulk Regulations Table has been determined using a Net Site Area of 470,812 square feet.
9. Prior to the Part II Approval (per Section 17-13-0610 of the Zoning Ordinance) for any portion of the site, the applicant shall submit a site plan, landscape plan and building elevations for the specific area or project phase for review and approval by the Department of Housing and Economic Development. Review and approval by the Department of Housing and Economic Development is intended to assure that specific development components substantially conform with the Planned Development and to assist the City in monitoring ongoing development. Site Plan Approval Submittals (per Section 17-13-0800) need only include that portion of the Property for which approval is being sought by the applicant. If the applicant is seeking approval for a portion of the Property that represents less than the entire site, the applicant shall also include a site

plan for that area of the Property which is bounded on all sides by either public or private rights-of-way or the boundary of the nearest building pad. The applicant shall coordinate final location of traffic signals with CDOT at time of Site Plan Review submittal. The site plan provided shall include all dimensioned and planned street rights-of-way.

No Part II Approval for any portion of the Property shall be granted until Site Plan approval has been granted. Following approval by the Department of Housing and Economic Development, the approved Site Plan Approval Submittals, supporting data and materials shall be made part of the main file and shall be deemed to be an integral part of the Planned Development.

After approval of the Site Plan Review, changes or modifications may be made pursuant to the provisions of statement Number 12. In the event of any inconsistency between approved plans and the terms of the Planned Development, the terms of the Planned Development shall govern. Any Site Plan Approval Submittals shall, at a minimum, provide the following information:

- fully-dimensioned site plan (including a footprint of the proposed improvements);
- fully-dimensioned building elevations and sections;
- fully-dimensioned landscape plan(s);
- statistical information applicable to the subject area, including floor area, the applicable FAR, uses to be established, building heights and setbacks;
- updated traffic study to be submitted, reviewed and approved by the Department of Transportation;
- final location of traffic signal to be approved by CDOT prior to site plan approval;
- provide pavement marking plan with parking restrictions if applicable and a fully dimensioned site plan identifying the public right-of-way features in the site plan i.e., sidewalk width, planter/parkway width, driveway width, and street width.

Site Plan Approval Submittals shall include all other information necessary to illustrate substantial conformance to the Planned Development.

10. Upon review and determination, "Part II Review", pursuant to Section 17-13-0610 of the Zoning Ordinance, a Part II Review fee shall be assessed by the Department of Housing and Economic Development. The fee, as determined by staff at the time, is final and binding on the applicant and must be paid to the Department of Revenue prior to the issuance of any Part II Approval.
11. The Site and Landscape Plans shall be in substantial conformance with the Landscape Ordinance and any other corresponding regulations and guidelines. Final landscape

plan review and approval will be by the Department of Housing and Economic Development. Any interim reviews associated with site plan review or Part II Reviews, are conditional until final Part II Approval.

12. The applicant shall comply with Rules and Regulations for the Maintenance of Stockpiles promulgated by the Commissioners of the Departments of Streets and Sanitation, Fleet and Facility Management, and Buildings, under Section 13-32-125 of the Municipal Code, or any other provision of that Code.
13. The terms and conditions of development under this Planned Development ordinance may be modified administratively, pursuant to Section 17-13-0611-A of the Zoning Ordinance by the Zoning Administrator upon the application for such a modification by the applicant, its successors and assigns and, if different than the applicant, the legal titleholders and any ground lessors.
14. The applicant acknowledges that it is in the public interest to design, construct and maintain the project in a manner which promotes, enables and maximizes universal access throughout the Property. Plans for all buildings and improvements on the Property shall be reviewed and approved by the Mayor's Office for People with Disabilities to ensure compliance with all applicable laws and regulations related to access for persons with disabilities and to promote the highest standard of accessibility.
15. The applicant acknowledges that it is in the public interest to design, construct, renovate and maintain all buildings in a manner that provides healthier indoor environments, reduces operating costs and conserves energy and natural resources. At the time of a hearing before the Chicago Plan Commission, all developments must be in substantial compliance with the current City of Chicago Sustainable Development Policy set forth by the Housing and Economic Development. The project shall meet basic LEED certification and 50 percent green roof of the Net Roof Area (81,000 square feet).
16. This Planned Development shall be governed by Section 17-13-0612 of the Zoning Ordinance. Should this Planned Development ordinance lapse, the Commissioner of the Department of Housing and Economic Development shall initiate a Zoning Map Amendment to rezone the Property to a C3-3 Commercial, Manufacturing and Employment District.

[Existing Zoning Map; Planned Development Boundary and Property Line Map;
Site Plan; Landscape Plan; Green Roof Plan; and North, South, East and
West Elevations referred to in these Plan of Development
Statements printed on pages 49234 through
49242 of this *Journal*.]

Bulk Regulations and Data Table referred to in these Plan of Development Statements reads as follows:

3/13/2013

REPORTS OF COMMITTEES

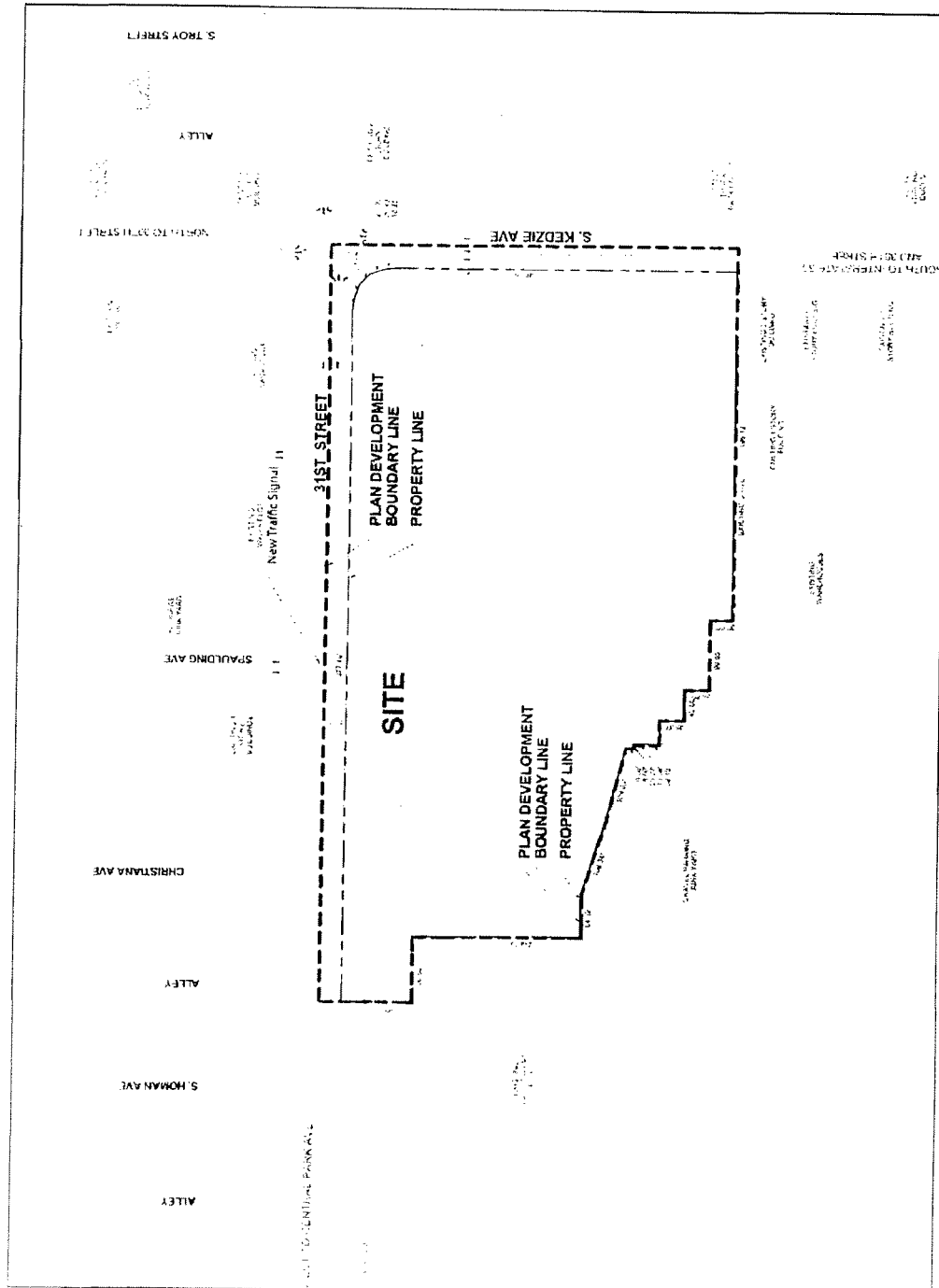
49233

*Institutional Business Planned Development.**Plan Of Development.**Bulk Regulations And Data Table.*

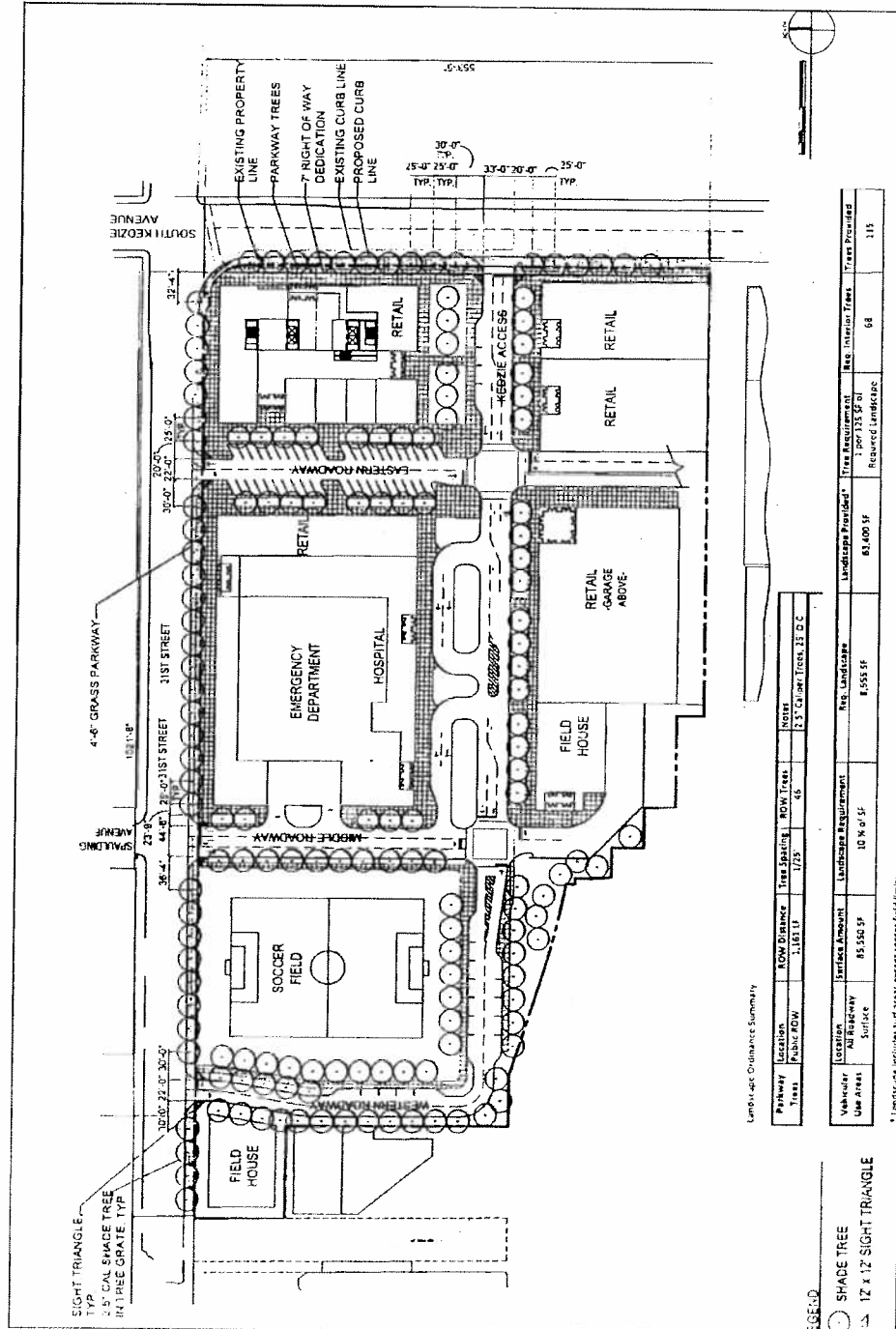
Gross Site Area = Net Site Area + Area remaining in the public right-of-way
526,185 square feet = 470,812 square feet + 55,373 square feet

Maximum FAR:	2.3
Setbacks From Property Line:	
31 st Street:	0 feet
Kedzie Avenue:	0 feet
Southern Property Line:	0 feet
West Property Line:	10 feet
Maximum Percentage of Site Coverage:	Per Site Plan
Maximum Number of Hospital Beds:	150
Minimum Number of Off-Street Parking:	
Hospital:	202 spaces
Medical Office:	178 spaces
Education:	50 spaces
Community/Meeting Space:	100 spaces
Day Care:	4 spaces
Recreation:	4 spaces
Retail	558 spaces
Minimum Total:	1,096 spaces
Minimum Number of Off-Street Loading:	6 berths (10 feet by 25 feet)
Maximum Building Height:	186.0 feet

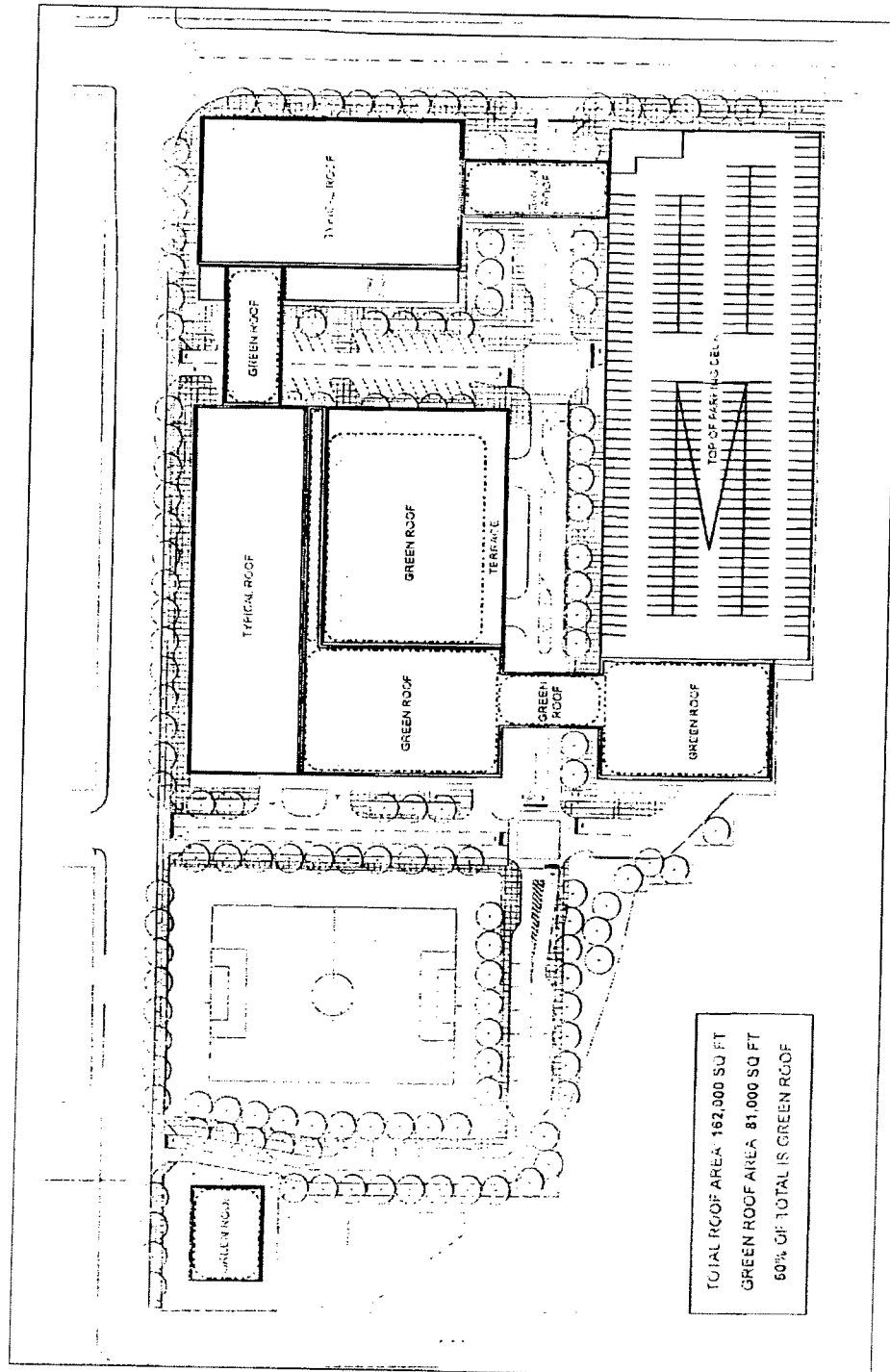
Planned Development Boundary And Property Line Map.



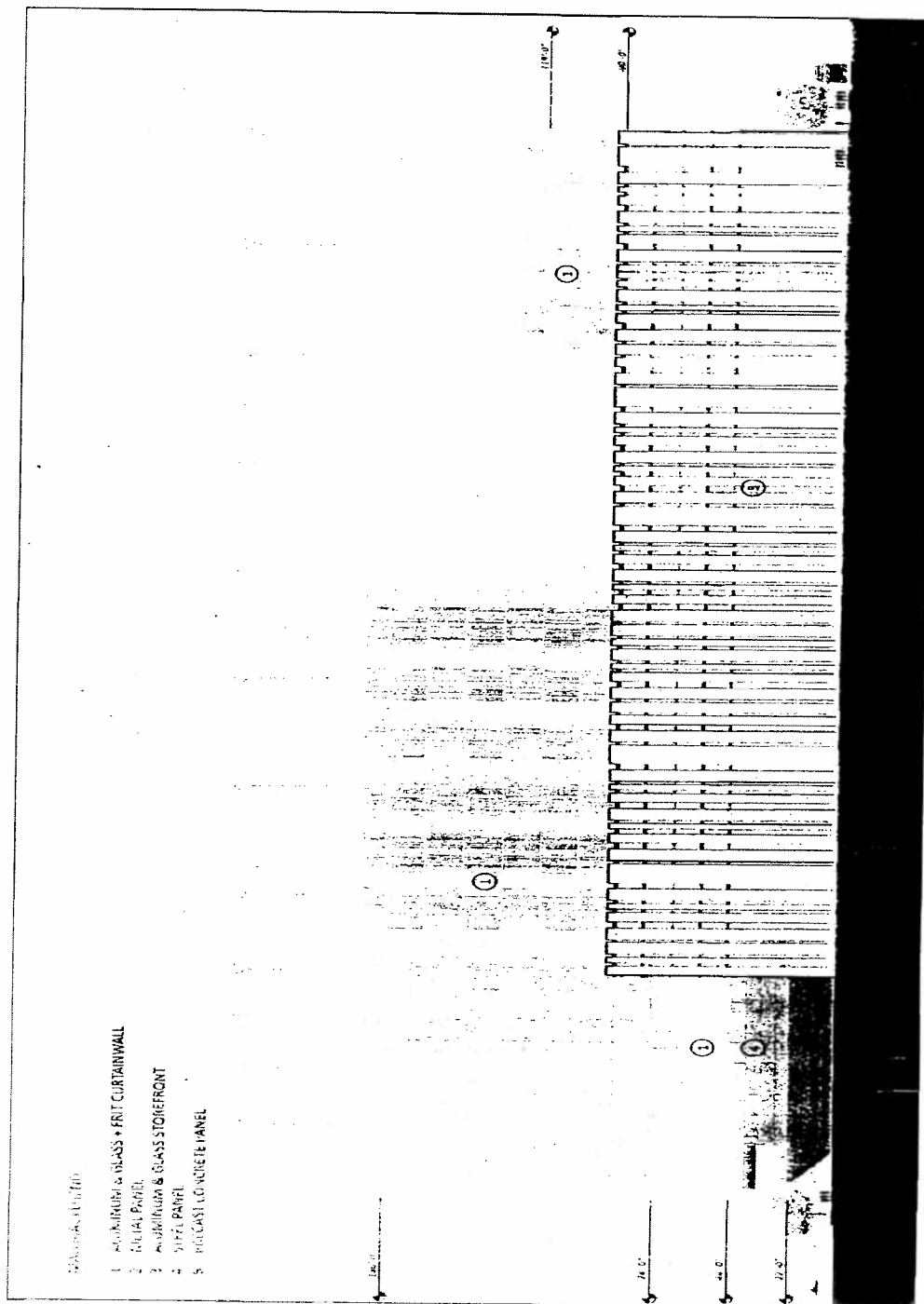
Landscape Plan.



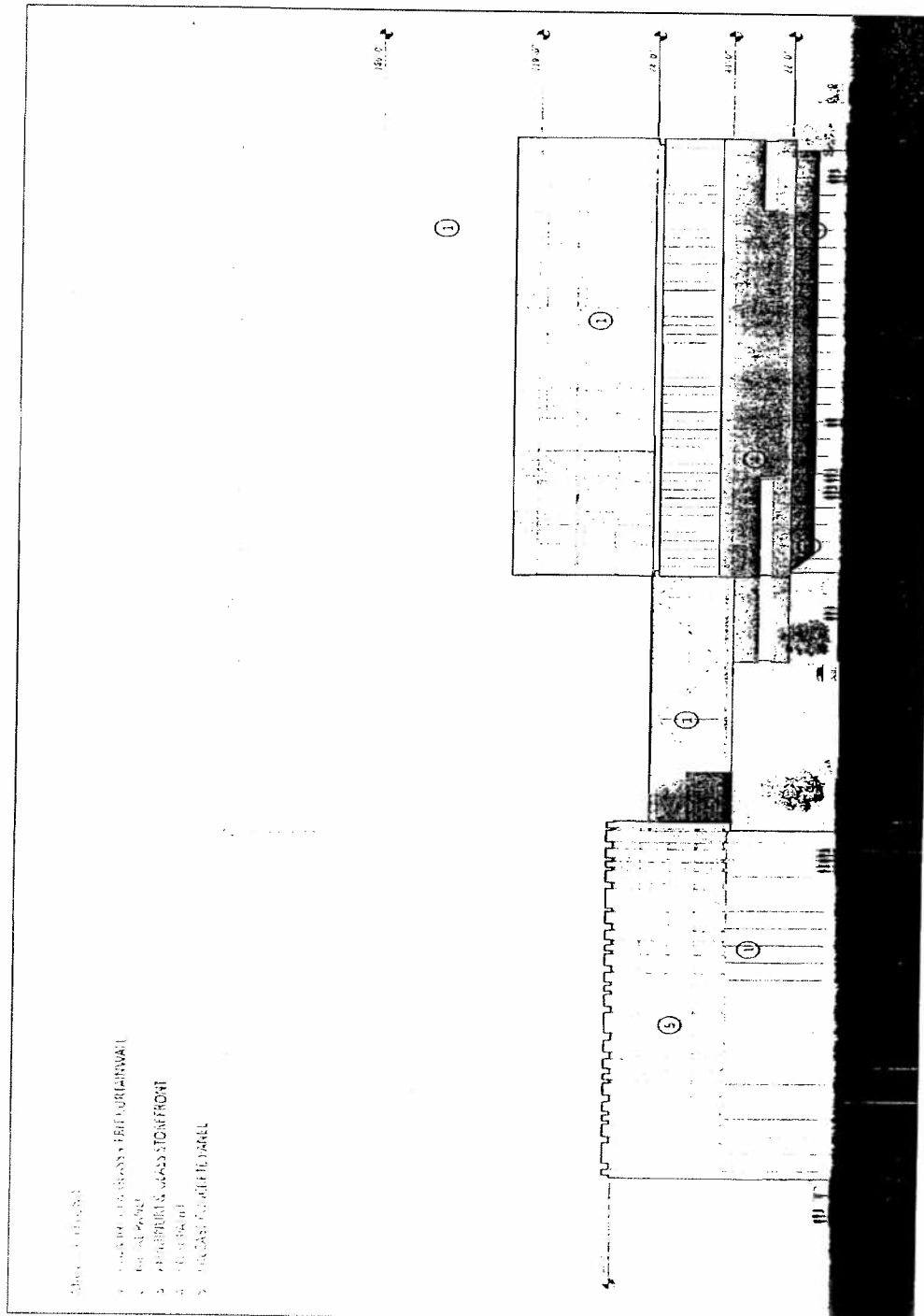
Green Roof Plan.



South Elevation.



East Elevation.



West Elevation.

